



**PLANNING COMMISSION MEETING AGENDA  
SILVERTHORNE TOWN HALL  
601 CENTER CIRCLE  
October 4, 2016 – 6:00 P.M.**

---

---

- 1. Call to Order**
- 2. Roll Call**
- 3. Consent Calendar ..... 1**  
Approval of the September 20, 2016, Planning Commission Meeting Minutes.
- 4. Citizen Comments**
- 5. Public Hearings:**
  - A. Minor Subdivision and Final Site Plan, Blue River Flats – 1056, 1088 and 1130 Blue River Parkway, Lots 23R and 25, Silverthorne Subdivision #1, and an unplatted tract described in warranty deed – Record No. 633874 ..... 3**  
The Applicant, Ken Marsh, Blue River 50, LLC, is requesting approval of a Minor Subdivision and Final Site Plan, to construct 46 condominium units in five, three-story buildings, and two additional garage structures.
- 6. Informational Items ..... 82**  
Upcoming Planning Commission items - Town Council Meeting Minutes: 9/14/2016; SPORT Committee: 9/15/2016 (not available at this time); EDAC Minutes: 9/5/2016.
- 7. Adjournment**



DRAFT

**TOWN OF SILVERTHORNE  
PLANNING COMMISSION MEETING MINUTES  
SEPTEMBER 20, 2016 – 6:00 P.M.**

---

**1. CALL TO ORDER** – The meeting was called to order at 6:00 p.m., September 20, 2016, in the Council Chambers of the Silverthorne Town Hall, 601 Center Circle, Silverthorne, Colorado.

**2. ROLL CALL** – Commissioners present and answering Roll Call were: Susan Byers, Donna Pacetti, Tim Nolan, and Jess Nelsen. Brian Wray, Jenny Gloudemans, Mike Bohlender, and Jen Stachelski were absent. Staff attending tonight’s meeting included: Matt Gennett, Planning Manager, Lina Lesmes, Senior Planner, Greg Roy, Planner I, and Melody Hillis, Planning Commission Secretary.

**3. CONSENT CALENDAR** – Tim Nolan made a motion to approve the September 6, 2016, Planning Commission minutes. Jess Nelsen seconded. The motion was approved by a vote of four zero (4-0). Mike Bohlender, Jenny Gloudemans, Jen Stachelski, and Brian Wray were absent.

**4. CITIZEN COMMENTS:**

None.

**5. ACTION ITEM:**

**A. Site Plan Modification, Rainbow Park West Entrance, 590 Rainbow Drive, Block A, Tanglewood Village, and two Unsubdivided parcels (TR5,78; Section 12; Quarter 1).**

Susan Miller Lee, Planner II, presented the project. The Applicant, the Town of Silverthorne, is requesting approval of a Site Plan Modification for the redevelopment of Rainbow Park’s west entrance.

**APPLICANT COMMENTS:**

Mark Wilcox - DHM Design, representing the Town of Silverthorne. Gave a presentation about the west entrance to Rainbow Park, history, public input and proposed improvements. Requested approval.

**COMMISSIONER COMMENTS:**

Jess Nelsen - Asked about the trees in the sight line, concerned about strollers, pedestrians, etc., being visible as they walk.

Mark Wilcox - Have tried to take all of that into consideration.

Jess Nelsen - Will there be quick drop off points/parking spots?

Susan Miller Lee - Yes.

Mark Wilcox - Showed the drop off spots on the renderings.

Susan Byers - Asked where they were exactly.

Mark Wilcox - Pointed out exactly on the renderings where the drop off points were located.

Tim Nolan - Will the existing trees on the berms have to be removed?

Mark Wilcox - May have to remove one or two, most will be saved.

Donna Pacetti - Bicycle lockup areas, where are those located?

Mark Wilcox - Showed on bicycle racks on the renderings.

Donna Pacetti - Still on street parking on Rainbow Drive?

Susan Miller Lee - Yes.

MIKE BOJLENDER ARRIVES AND IS SEATED AT 6:21 PM.

**PUBLIC COMMENT:**

- Jennie Gladen - Resident of the Town/4<sup>th</sup> Street. Asked if in addition to the men’s and women’s restroom, if there was going to be a family restroom provided.
- Mark Wilcox - Matching the design of the Tot Lot, doesn’t believe there will be a family restroom provided.
- Jennie Gladen - Would like to see the Town take the under consideration.

**PUBLIC COMMENT CLOSED**

**COMMISSIONER COMMENTS:**

None.

**SUSAN BYERS MADE A MOTION TO APPROVE THE SITE PLAN FOR THE RAINBOW PARK WEST ENTRANCE, LOCATED AT 590 RAINBOW DRIVE, AS RECOMMENDED BY THE SPORT COMMITTEE.**

**TIM NOLAN SECONDED.**

**MOTION PASSES BY A VOTE OF FOUR TO ZERO (4-0). JENNY GLOUDESMAN, JEN STACHLESKI AND BRIAN WRAY WERE ABSENT. MIKE BOHLENDER ABSTAINED DUE TO NOT BEING PRESENT DURING PRESENTATION.**

**6. INFORMATIONAL ITEMS:**

Matt Gennett – Blue Shores, LLC (Silvertrout) is back in for review. Blue River Flats is in process for final site plan.

**7. ADJOURNMENT:**

**TIM NOLAN MADE A MOTION TO ADJOURN.**

**SUSAN BYERS SECONDED. MOTION PASSES BY A VOTE OF FIVE TO ZERO (5-0). JENNY GLOUDEMANS, JEN STACHELSKI AND BRIAN WRAY WERE ABSENT.**

**8. WORKSESSION ITEM:**

**Discussion on the 2016 Sign Code Updates.**

Submitted for approval by:

4<sup>TH</sup> day of October, 2016.

\_\_\_\_\_  
Melody Hillis, Planning Commission Secretary

\_\_\_\_\_  
Brian Wray, Planning Commission Chairman

These minutes are only a summary of the proceedings of the meeting. They are not intended to be comprehensive or to include each statement, person speaking or to portray with complete accuracy. The most accurate maintained in the office of the Planning Commission Secretary.

**Town of Silverthorne, Colorado**  
**Planning Commission Staff Report**

**From:** Lina Maria Lesmes, AICP, Senior Planner *LML*

**Through:** Matt Gennett, AICP, Planning Manager *MG*

**Date:** September 29, 2016, for meeting of October 4, 2016

**Subject:** Minor Subdivision and Final Site Plan for Blue River Flats (PT2016-5)

**Owner/Applicant:** Ken Marsh – Blue River 50, LLC

**Agent:** Lindsay Newman – Norris Design, Inc.

**Proposal:** The applicant is requesting approval of a Minor Subdivision and Final Site Plan to combine three existing parcels, modify easements, and construct 46 condominium units in five, three-story buildings. Two additional garage structures are also proposed.

**Address:** 1056, 1088 and 1130 Blue River Parkway

**Legal Description:** Lots 23R and 25 Silverthorne Subdivision #1, and an unplatted tract described in a warranty deed, Record No. 633874.

**Site Area:** Lot 1: 91,921 square feet or 2.11 acres  
Tract A: 22,148 square feet or 0.51 acres

**Zone District:** Riverfront Zone District (RF)

**Design District:** Riverfront Design District

**Site Conditions:** The three parcels are currently developed with three single family homes that were constructed in the 1960s.

**Adjacent Uses:** North: Open Space, Town owned - vacant  
South: Multi-Family development (RF)  
East: Willowgrove Subdivision (R15)  
West: Blue Sky Building and Silverthorne Mini Storage (C1)

**Height:** Allowed: 35 feet  
Proposed: 35 feet

**Lot Coverage:** Allowed: 60% of lot area or 55,153 square feet  
Proposed: 34% of lot area or 32,000 square feet

<b>Parking:</b>	Required:	92 plus 4 handicap accessible spaces	
	Proposed:	92 plus 4 handicap accessible spaces	
<b>Snow Storage:</b>	Required:	25% of paved area or 6,225 square feet	
	Proposed:	25% of paved area or 6,537 square feet	
<b>Landscaping:</b>	Required:	80 trees and 131 shrubs	
	Proposed:	80 trees and 154 shrubs	
<b>Setbacks:</b>	Front	Required:	50% 0' setback, 50% 5' setback
		Proposed:	13% 0' setback, all other 5' setback
	Rear	Required:	25 feet from top of bank of Blue River
		Proposed:	25 feet from top of bank of Blue River
	Side	Required:	5 feet
		Proposed:	5 feet

**BACKGROUND:** The applicant, Blue River 50, LLC, purchased Lots 23R, 25, Silverthorne Subdivision #1, and the unsubdivided tract, on August 17, 2015. It is his intent to develop the parcels as a high-density residential project with 46 condominiums in 5 buildings, located adjacent to the Blue River. Parking for the project is composed of surface parking, understructure garage parking, and separated garage parking. Two access points are proposed from HWY 9, and connected via an internal drive.

**PREVIOUS COUNCIL ACTION:** The plat of the Silverthorne Subdivision #1 was approved in 1957, and created 25, 100-foot wide lots along HWY 9, and 22 lots along Annie Road. On January 22, 2003, Town Council denied a Sketch PUD, Subdivision, and Site Plan for a mixed-use project entitled 'Sapphire Plaza' proposed on Lots 23 and 24, Silverthorne Subdivision #1. On March 24, 2004, Town Council approved a lot line vacation between Lots 23 and 24, Silverthorne Subdivision #1, thereby creating Lot 23R.

After several Sketch proposals, Town Council approved a Final Commercial Subdivision, Final Site Plan, and Wetland Disturbance Permit for Sapphire Plaza III on December 10, 2008. The project consisted of 17 residential units and 4,543 square feet of commercial space on the three parcels in the subject application. The approval expired for lack of action in 2009.

On June 22, 2016, Town Council approved the Blue River Flats Preliminary Site Plan with the following conditions:

1. That no building or portion of a building encroach onto any easement on the property.
2. That a Minor Subdivision Plat be submitted with the Final Site Plan to modify any of the existing easements, to create a 25-foot pedestrian path easement, and to combine the three lots on which the project is proposed.
3. That the trail in the rear of the property and the sidewalk in the front of the property be

constructed prior to the issuance of a Certificate of Occupancy.

4. That the driveways be revised such that the width at the property line does not exceed 24 feet.
5. That the south driveway be revised such that it is no closer than 5 feet from the south property line, and it is defined by concrete curb and gutter.
6. That an access easement be provided along the south property line to facilitate a future shared access with Lot 22, Silverthorne Subdivision #1.
7. That the FEMA approved CLOMR-F be submitted with the Final Site Plan application.
8. That the Utility Plan be revised such that all utility lines on the property are undergrounded, as required by Section 4-5-5(3).
9. That the privacy fence proposed along the south property line be revised such that it is not within the required 25-foot pedestrian path easement in the rear, or in a location that might obstruct visibility of drivers entering and exiting the site.
10. That a sidewalk be proposed accessing the northernmost garage building, and the door that accesses the interior handicap parking space in that building.
11. That all utility, telecommunications, ground mounted, roof top mechanical equipment, and electrical boxes be shown on the elevations of the Final Site Plan.
12. That the garage buildings be revised to meet Standard 4.2.5 of the Riverfront Design District Standards and Guidelines, which prohibits building facades from exceeding a length of 50 feet without a change in plane.
13. That additional architectural variation be provided for the garage buildings to meet Standard 4.2.6 of the Riverfront District Design Standards and Guidelines.
14. That additional roof elements be provided for the garage buildings to meet Standard 4.5.2 of the Riverfront District Design Standards and Guidelines.
15. That the comments of the Town Engineer be addressed with the Final Site Plan.
16. That the comments of the Utilities Manager be addressed with the Final Site Plan.

**STAFF COMMENTS – MINOR SUBDIVISION:** The intent of the Minor Subdivision is to provide a one-step review process for the creation of 4 or fewer lots, or parcels of land. Section 4-5-11.d. requires that Minor Subdivision plats be consistent with the Comprehensive Plan, be consistent with Chapter 4 of the Code, meet all the public notice requirements, and execute a Subdivision Improvements Agreement (SIA).

1. **Consistency with Comprehensive Plan:** Policies in the Comprehensive Plan support high density residential uses in the Riverfront District, the combination of narrow lots for cohesive development, and the dedication of privately owned river property. The proposed Minor Subdivision is consistent with those policies, and in general conformance with the Comprehensive Plan.
2. **Consistency with Chapter 4, Article IV:** The second criterion for Minor Subdivisions is consistency with Chapter 4 of the Code, Article IV, Zoning Districts.

All of the Zoning Standards in Section 4-4-13, RF, Riverfront Zone District regarding use, lot frontage, lot size, and setbacks are met with the proposed plat. The subdivision creates Lot 1, with an area of 2.110 acres, as the unified lot for the condominium project and its associated site plan requirements for parking, landscaping, and snow storage. The subdivision also creates Tract A, which consists of a portion of the Blue River, and which is proposed to be dedicated to the Town. Lastly, the plat relocates utility easements, creates an exclusive water and sewer line easement, an access easement, and a 25-foot Multi-Use Recreational Trail easement.

- 3. Consistency with Chapter 4, Article V:** The third criterion for Minor Subdivisions is consistency with Chapter 4 of the Code, Article V, Subdivisions.

Section 4-5-11.f, Minor Subdivision Plat Exhibit, details the required items that must be shown on a Minor Subdivision Plat. All of the requirements regarding titles, boundaries, scale, certificates, legal descriptions, ties to surveyed monuments, and the acreage are met. In conformance with Section 4-5-11.g, Final Development Reports and Plans, the applicant submitted the following supporting materials, which were necessary for review of the Minor Subdivision and Site Plan:

- Final Drainage Report and Drainage Plan
- Traffic Impact Analysis
- Floodplain Analysis
- FEMA Conditional Letter of Map Revision based on Fill (CLOMR-F)
- Final utility construction plans and profiles
- Construction cost estimates for public improvements
- Wetland Delineation and Verification from the USACE
- CDOT Access Permits

Copies of all these technical plans and reports are available for review in the Community Development Department office.

- 4. Public Noticing:** In conformance with Section 4-7-3, Public Hearing Notice Requirements, a public notice regarding the Planning Commission and Town Council public hearings appeared in the Summit Daily News on September 26, 2016, and a mailed notice was sent to adjacent and contiguous property owners.
- 5. Subdivision Improvements Agreement:** For Minor Subdivisions, Section 4-5-11.d.5 requires that the applicant execute a Subdivision Improvements Agreement (SIA) for all public improvements needed to mitigate the impacts of the subdivision. An SIA has been prepared by the Town for the project's public improvements, including the sidewalk along HWY 9, the water and sewer main lines, and the fire hydrants. A letter of credit will be required secure the performance and completion of the improvements in an amount equal to 150% of the estimated cost of said improvements. Such letter of credit will be due to the Town prior to recording the Minor Subdivision Plat and the SIA.
- 6. Public Dedications:** Staff finds that all applicable public dedication requirements have been satisfied. Tract A is being dedicated to the Town via the plat.

**STAFF COMMENTS – FINAL SITE PLAN:** The Site Plan review process is reviewed by Planning Commission and Town Council to determine if the plan complies with the Comprehensive Plan, Code requirements, and Design District Standards.

**Comprehensive Plan:** The first criterion for review of a Final Site Plan is consistency with the goals and policies of the Comprehensive Plan. The following policies apply:

*Policy LU 1.6 – Ensure that all development is integrated with multi-modal transportation options and provides for safe connections between destinations for all users.*

*Policy LU RF 1 – Encourage high density residential development in the Riverfront District that is complementary to its river-oriented location, is well-connected to adjacent commercial and civic uses, maintains public connection to the riverfront, and adds a unique dimension to housing options in Silverthorne.*

*Policy LU RF 2 – Encourage the combination of 100-foot wide lots for cohesive development within the Riverfront District. Town-owned lots in this area may be utilized to offset on-site parking requirements.*

*Policy LU RF 3 – Support site designs that are human-scaled and pedestrian oriented, and that offer a convenient option for pedestrian movement within and between developments.*

*Policy LU 4.4 – Encourage the dedication of privately-owned river property to the Town for public access.*

*Policy LU 5.6 – Preserve and protect areas of significance to the community. These include, but are not limited to, wetlands, steep slopes, 100-year floodplain, significant landforms, significant vegetation, and view corridors.*

*Policy LU 5.9 – Require new and existing developments to provide adequate measures to control, manage, and minimize adverse effects on the water resources and water quality of the region. These include items such as water related treatment and management basins and/or plans for storm-water and water quality management.*

*Policy LU 5.11 – Ensure that development adjacent to the Blue River is accomplished in a manner that enhances the river corridor, protects water quality, and maintains pedestrian access and use.*

*Policy CD 3.2 – Encourage the use of natural and long-lasting materials and colors that are enduring and complementary to Silverthorne’s natural setting and existing development.*

*Policy CD 3.3 - Building mass, form, length, and proportions shall be designed to provide variety of visual interest, maintaining a human scale that is appropriate to surroundings.*

*Policy T 1.5 – Limit vehicular access to HWY 9, with major traffic generators using secondary access points rather than direct highway access whenever possible.*

*Policy T 2.6 – Encourage development to provide bicycle parking facilities, and improve bicycle parking at bus stops and areas of public activity.*

Staff finds that the proposed development is consistent with several policies of the Silverthorne Comprehensive Plan. The high density development in the Riverfront District combines narrow lots, provides additional housing options, and maintains a public connection to the riverfront. The project also proposes drainage improvements and water quality basins that serve to minimize adverse impacts to the Blue River. The buildings are designed with elements scaled to the pedestrian, and with natural and enduring materials. Bicycle parking, pedestrian connections, and relationship to the Blue River are all aspects of the project, addressing the goals and policies of the Comprehensive Plan.

**Chapter 4 of the Town Code:** The second criterion for review of a Final Site Plan is consistency with Chapter 4 of the Town Code, particularly Article IV, Zoning Regulations, and Article VI, Site Plan.

**Zoning Standards** – The Riverfront Zone District (RF) is envisioned as a high density residential area with distinct character and a strong relationship to the Blue River. Staff finds that the application meets the RF Zone District requirements for land use, lot frontage, and lot coverage. The remaining zoning standards are evaluated below.

**Density** – The RF zoning permits a maximum of 25 dwelling units per acre. For a lot area of 2.11 acres, 53 residential units are permitted. The applicant is proposing 46 residential units, not exceeding the density maximum.

**Setbacks** – For 50% of the HWY 9 lot frontage, a 0-foot setback is permitted. For the remaining 50%, a minimum 5-foot setback is required. The lot frontage along HWY 9 extends for approximately 590 feet. Approximately 75 feet of combined building lengths is proposed to meet the 0-foot setback. The remaining portions of the structures are a minimum of 5 feet from the front property line. In addition, all structures meet the 5-foot side setback requirement, and the 25-foot setback from the top-of-bank of the Blue River.

**Height** – The maximum building height in the Riverfront Design District is 35 feet, as measured from the average existing grade underneath each building. Below is a summary of the heights for the Blue River Flats buildings.

Building	Average Existing Grade	Highest Ridge Elevation	Height (feet)
1	8,708	8,743	35
2	8,708.75	8,743.24	34.5
3	8,708.25	8,743.04	34.8
4	8,707.1	8,741.49	34.4
5	8,707.5	8,742.41	34.9

The five condominium buildings are proposed to be raised above the elevation of the floodplain, but will not exceed the height maximum. The two garage buildings measure 13.6 feet, within the maximum height allowance.

**Pedestrian Access and Circulation** – The Riverfront Zone District has three path connection requirements. The first requires that a pedestrian path be constructed within

the riverside setback, in accordance with the Town Parks, Trails, and Open Space Master Plan. Per that document, the pedestrian path is required to be 10-feet wide, and finished with asphalt. To meet the condition of approval of the Preliminary Site Plan, the applicant is proposing a 10-foot asphalt path along the Blue River, meeting the standard.

The second requirement is for a minimum 6-foot wide concrete sidewalk to be located along the property line parallel to HWY 9. To meet the condition of approval of the Preliminary Site Plan, the applicant is now proposing a 6-foot wide concrete sidewalk along the HWY 9 frontage. The applicant has also obtained approval from CDOT for locating this sidewalk within their right-of-way.

Lastly, the applicant is required to provide a 25-foot pedestrian path easement within the setback from the top of bank of the Blue River. Such easement is shown on the proposed Minor Subdivision plat. Staff finds that all the pedestrian circulation requirements of the Riverfront Zone District are met with the Final Site Plan.

***Vehicular Access and Circulation*** – Adjacent to the subject site, HWY 9 is a four-lane, median-divided roadway with left turn lanes for northbound and southbound traffic at 12<sup>th</sup> Street. The next full movement intersection is located at 10<sup>th</sup> Street, approximately 350 feet south of the project site. The project is proposed to take access from HWY 9 via a right-in/right-out (RIRO) driveway on the south, and a full-movement driveway aligned with 12<sup>th</sup> Street, on the north.

Driveway Widths – To address the condition of approval of the Preliminary Site Plan, the applicant has revised the driveway widths such that they do not exceed the maximum width of 24 feet allowed at the property line.

South Driveway – Currently, there is an unofficially shared driveway on the south that straddles the property line between Lots 23R and Lot 22. The owner of Lot 22, Betty Boatman, has indicated that she is not interested in creating an officially shared driveway with the Blue River Flats development.

The Town Code has the following standards for driveways:

- Section 4-6-10.e.4.c – Frequency of Driveways – *“No two driveways connecting a public street, alley, or highway to an off-street parking area shall be within 30 feet of one another at their intersections with the property line.”*
- Section 4-6-10.e.4.l – Curb – *“Concrete curb and gutter required. All off-street parking surfaces shall be defined by concrete curb and gutter (for all uses).”*
- Section 4-6-10.e.4.m –Location – *“Facilities serving residential or commercial uses shall not be closer to the property line than 10 feet.”*

The Public Works Department can approve a reduction in the standards above if it finds that a requirement is not justified by the characteristics of the proposed driveway. With the review of the Preliminary Site Plan, Staff found that it was appropriate to waive the frequency of driveways standard.

It is the Town's long-term goal to minimize curb cuts on HWY 9 and promote shared access drives in the Riverfront District. As such, as a condition of approval of the Preliminary Site Plan, the Town permitted a reduced setback width of 5 feet for the south driveway if the applicant provided an access easement to facilitate a future shared access configuration with Lot 22, to the south. Moreover, the 5-foot setback provided the minimum space necessary for functional snow storage.

The Final Site Plan proposes the south driveway 5 feet from the property line, defined in its entirety by concrete curb and gutter. The Minor Subdivision plat also proposes a 24-foot access easement from the south property line, meeting the Town's requirements.

**Parking** – The project proposes 46 residential condominiums of various sizes. The table below provides Staff's calculations on the amount of parking required, and the reductions permitted by Section 4-6-10.f.5.

Use	Minimum Parking Required	Available Reductions
10, 1BR condominiums	15	Bicycle Parking – 4.8
35, 2BR condominiums	70	
1, 3BR condominiums	2.5	
Visitor Parking	9.2	
Handicap Parking	4	None
<b>Parking Required Onsite: 92 + 4 HC spaces</b>		

The plans indicate that there are 41 enclosed garage parking spaces, 51 surface spaces, and 4 handicap spaces. All exterior parking facilities are shown a minimum of 10 feet from a property line, and defined by curb and gutter, meeting the Code requirements.

To qualify for the bicycle parking reduction, 35 bicycle parking spaces are necessary. Sheet L-2 shows 3 exterior bicycle racks, and one interior bicycle storage space, with room for 37 bicycles, meeting the requirements.

**Traffic** – Felsburg Holt & Ullevig Traffic Consultants prepared a Traffic Assessment for Blue River Flats on January 20, 2016. It is noted in the Assessment that the Blue River Flats project will generate approximately 350 vehicle daily trips (considered relatively low). Based on that volume of traffic, new left or right-turn lanes are not deemed necessary to accommodate the Blue River Flats project. The Assessment states that, *“in regards to the projected traffic volume levels, auxiliary lane requirements, and sight distance availability, the construction of the Blue River 50 will have very little impact on the overall driving experience of existing motorists along HWY 9.”* CDOT reviewed the Traffic Assessment, and agreed with the design recommendations for the access points.

**Snow Storage** – A minimum of 25% of the total paved area must be provided for snow storage. Sheet L-3 indicates that there are 24,899 square feet of paved areas, requiring 6,114 square feet of snow storage. The applicant is proposing 6,537 square feet of snow storage in locations that facilitate the snow stacking process, meeting the requirement.

**Landscaping** – The site encompasses 91,921 square feet. With a 5% reduction for incorporating xeriscaping practices, 87 trees (50% deciduous and 50% coniferous) and 131 shrubs are required. Up to 10% of the required number of trees may be substituted for alternative forms of landscaping, including benches. The applicant is proposing 7 benches, reducing the required number of trees to 80. Sheets L-1 and L2, Landscape Plan, indicate that 46 deciduous trees, 34 coniferous trees, and 154 shrubs are being proposed, meeting the minimum requirements. As part of the Landscape Plan, the applicant is also proposing a variety of benches along the riverside pathway, ornamental grasses, and a ‘community green’ gathering space for the residents in the development

As a condition of approval, the Landscape Plan must be modified such that the 50/50 ratio of coniferous to deciduous trees is addressed with the building permit submittal.

**Wetlands** – The combined sites contain 0.4 acres of wetlands, which were delineated in October of 2015, and are shown on the Site Plan and Existing Conditions Map. The US Army Corps of Engineers has verified the location of the wetlands. With the proposed subdivision, the wetlands will be located entirely on the Tract A, which is proposed to be dedicated to the Town. Section 4-8-19 requires a 10-foot buffer zone from wetland areas for the subject property. Wetland areas are not proposed to be disturbed with the proposed development. The only disturbance in the buffer zone will be the planting of trees, shrubs, and revegetation materials.

**Floodplain Development Standards** – The Blue River Flats property is within FEMA’s designated Zone AE, which is subject to inundation by the 100-year based flood (1% annual chance flood). The limits of the 100-year floodplain are shown on the Site Plan.

The floodplain management regulations are detailed in Chapter 3, Public Works, Article VIII, Flood Damage Prevention. The Town Engineer acts as the Floodplain Administrator, designated to enforce the floodplain management regulations. Please see Exhibit H for details on the Town’s Engineer’s analysis of the floodplain standards.

**CLOMR-F/LOMR-F** – Computer models of the floodplain are used to simulate “encroachment” or fill in the flood fringe in order to predict where and how much the Base Flood Elevation (BFE) would increase if the floodplain is allowed to be filled. The applicant submitted a Floodplain Analysis (Exhibit F) prepared by Wright Water Engineers on February 15, 2016, which modeled the potential changes to the BFEs due to the proposed development, and evaluated the impacts to surrounding properties.

Based on this analysis, the applicant obtained a Conditional Letter of Map Revision based on Fill (CLOMR-F) through FEMA. The CLOMR-F is a letter from FEMA acknowledging that proposed structures would not be inundated by the 100-year flood if fill is placed and the structures are built as proposed. The Letter of Map Revision Based on Fill (LOMR-F) is an official change to a FIRM that is issued after the structures are built to document FEMA’s determination that the structures are no longer in the Special Flood Hazard Area (SFHA). The issuance of a LOMR-F eliminates the federal flood insurance requirement for properties located within the 100-year floodplain.

**Drainage** – Stormwater drainage from the commercial properties west of HWY 9 flows through a pipe underneath the highway, to a roadside ditch west of the subject property, and into the Blue River. As part of the Blue River Flats project, the applicant is proposing to improve the roadway ditch, and continue to direct that drainage to the north.

Stormwater drainage generated by the project is proposed to be conveyed by curb and gutters into a concrete pan in the center of the internal drive, to a water quality pond located on the north end of the property, and finally to the Blue River. Runoff from the rooftops closest to the Blue River is designed to flow overland on native slopes, then into the Blue River. The Drainage Plan emphasizes water quality, but also provides storm water detention capacity. The Town Engineer has reviewed the Drainage and Grading Plan, and finds that it meets the Town's requirements.

**Utilities** – Existing water and sewer mains run within the HWY 9 right-of-way. The applicant is proposing water and sewer main connections underneath the private drive, within exclusive water and sewer line easements. Electric lines, transformers, and gas lines are also located within the HWY 9 right-of-way. Overhead utility lines, a power pole, and associated guy wires currently exist on the property.

Section 4-5-5 (3) requires that all utilities on the property be undergrounded. Specifically, *"no subdivision shall be approved unless the plans and specifications provide all utilities to be placed underground."* As a condition of approval of the Preliminary Site Plan, the applicant was required to revise the plans such that all utilities, including those across the Blue River, were undergrounded. Nonetheless, the current plans propose to relocate the utility pole towards the east, underground the electricity lines that extend west, but not those that cross the Blue River to the east.

The applicant explored 3 options for undergrounding the utilities, including routing them to the Tammy Jamieson bridge, building a bridge across the Blue River, and undergrounding the utilities under the river (Please see Exhibit E for details). Based on that analysis, the applicant concluded that undergrounding the utilities was cost prohibitive, and would significantly delay construction of the project.

As a compromise, the applicant is proposing to dedicate Tract A to the Town, and to create a funding mechanism for undergrounding the utilities in the future. The proposal is to record a Real Estate Transfer Assessment (RETA) that would require 1% of each real estate transfer to be submitted to the Town in perpetuity. Other subdivisions in Town that have agreed to a RETA include Angler Mountain Ranch, Maryland Creek Ranch, and Brian Avenue Business Court. Lastly, the applicant has requested that the first sale of each of the 46 condos be exempt from the RETA. Please see Exhibit F for details.

Staff finds that the dedication of Tract A, and the proposed funding mechanism (RETA) for undergrounding the utilities at a later date is a benefit to the Town. Additional details on the RETA and the revenues that might be generated over time will be provided as part of the Condominium Subdivision Plat that will be required prior to the sale of any units.

**Dumpster Enclosure** – Trash and recycling facilities are located within the building on the west side. There is adequate space for a garbage truck to enter and exit the site in a forward-flow motion.

**Lighting** – Lighting information, including exterior light fixture cut sheets are shown on Sheet E.01. Bollard lights are proposed along the pedestrian walkways, as well as down-cast wall mounted fixtures at building entrances. The applicant is also proposing 10 pole mounted light fixtures (S1). Two S1 fixtures are shown off-site. The Town Code requires all project light fixtures be onsite, and setback from the property lines at least as tall as the fixture height. As a condition of approval, all pole-mounted light fixtures must be located a minimum of 14 feet (proposed height) from the property lines.

**Riverfront District Design Standards and Guidelines:** The third criterion for Final Site Plans is consistency with the district design standards and guidelines. Standards are baseline requirements. Guidelines are recommendations that are intended to further define the desired character of development in the Riverfront District.

#### ***Building Orientation and Location on Site***

Guideline 3.1.1 states, “*Development that creates a ‘wall’ that blocks access to the Blue River, or access between developments is discouraged.*” The applicant is proposing a 6-foot cedar fence along the south property line to provide privacy for the neighbor to the south. In response to the condition of approval of the Preliminary Site Plan, the applicant revised the fence such that it no longer within the Multi-Use Recreational Trail Easement in the rear, and is setback 20 feet from the front property line. Staff finds that the applicant has adequately addressed this guideline.

Guideline 3.1.3 states, “*Architectural elements that enhance the pedestrian environment and create an inviting atmosphere, including balconies, canopies, porches, patios, creative walkways, and gathering spaces are encouraged. Orientation of such elements should prevent the shedding of snow onto pedestrian areas.*” These design elements have been incorporated into the project, addressing the guideline.

#### ***Pedestrian Access and Circulation***

Standard 3.2.1 states, “*The Blue River Path is an important focus of the Riverfront District. Developments shall provide a trail connection along the river, in accordance with the Silverthorne Parks, Trails, and Open Space Master Plan and the Town Code, as well as interconnections between adjacent sites.*” As noted previously, a pedestrian path is proposed along the river. Because interconnections between adjacent sites is required by this standard, Staff supports the revised length for the fence.

Standard 3.2.2 states, “*Where a public sidewalk is deemed necessary by the Town, it shall be installed in the public right-of-way as part of the proposed development.*” In order to meet this standard, the applicant has revised the plans to propose a sidewalk along HWY 9, meeting this standard.

Standard 3.2.3 states, “*Continuous internal pedestrian walkways within a development*

site, not less than 6 feet in width, shall be provided from the principal building entrance to adjacent sidewalks, trails, and public right-of-ways". The internal walkways meet this standard.

### ***Vehicular and Service Area Access and Circulation***

Standard 3.3.1 states, "*Vehicle circulation onsite shall be clearly organized to facilitate movement into, and throughout, parking areas. Parking drive lanes and intersections shall align wherever practical.*" Vehicular circulation is functional and organized, and parking areas are designed with adequate space to maneuver. The applicant is proposing to align the north driveway with 12<sup>th</sup> Street in order to offer full turn movements in and out of the site. This standard is met.

Guideline 3.3.2 states, "*Contiguous developments are highly encouraged to combine access points to minimize curb cuts, and to provide connections between adjacent properties.*" In response to a condition of approval of the Preliminary Site Plan, the applicant has proposed a 24-foot access easement on the south side. This will ensure that there is the potential to create a future shared access with the property to the south.

Guideline 3.3.3 states, "*The consolidation of adjacent lots is strongly encouraged to create cohesive development projects. Developments should consider cross access drives that may provide shared access to multiple properties.*" The applicant is proposing to consolidate 3 existing lots with the development of the Blue River Flats. A shared access with the property to the south was explored to address this guideline.

### ***Parking***

Standard 3.4.1 states, "*Parking areas shall be located so as to minimize visual and noise impacts on adjacent properties and the public right-of-way.*" A large portion of the parking is located within garages. Surface parking is proposed in the interior of the site, minimizing the visual and noise impacts, meeting this standard.

Guideline 3.4.3 states, "*Parking within buildings, including parking located in architecturally integrated garages, is encouraged to minimize the amount of uncovered surface parking.*" Approximately half of the required parking on the property is located in architecturally integrate garages, addressing this guideline.

### ***Landscape***

Standard 3.5.1 states, "*Development must provide landscaping that addresses and complements the recreational aspect of the Blue River. To that effect, the natural vegetation surrounding the river should be preserved, to the greatest extent feasible.*" The applicant is proposing sufficient landscaping to meet the Town Code, and is not disturbing the natural vegetation around the river. A note on the landscape plan indicates that existing trees will be preserved where possible, meeting this standard.

Standard 3.5.3 states, "*Significant landscape materials, such as trees, shall be located outside utility easements. Planting of trees over utility lines is prohibited.*" Trees are not

proposed within utility easements, meeting this standard.

Guideline 3.5.5 states, “*Landscaped neighborhood gathering spaces, including courtyards, mini parks, neighborhood greens, and playgrounds are encouraged within high density residential developments.*” The applicant is proposing a ‘community green’ as part of the Landscape Plan, addressing this guideline.

### **Screening**

Standard 3.7.2. Refuse, Recycling, Storage and Service Areas states,

- a. *“Refuse, recycling, and service areas shall be located to the rear or side of buildings, or in other inconspicuous locations, where they are generally not noticeable from public rights-of-way, pedestrian walkways, or open spaces.*
- b. *All outdoor refuse, recycling containers, and dumpsters shall be screened from view from adjacent properties and public rights-of-way by enclosure in a permanent, four-sided, solid, and opaque structure with a roof.*
- c. *Refuse, recycling, storage, and service structures shall be designed in the same architectural style and be constructed of materials and colors complementary to the primary building on site.*
- d. *All outdoor storage of materials, vehicles, and/or ancillary equipment shall be screened from public view with a permanent, solid, opaque fence; or with landscaping designed to create year-round screening. Fences shall be designed to match the architecture of the primary structure.”*

Trash and recycling containers are proposed in a four-sided enclosure with a roof, located along the internal drive. The structure is constructed of the same materials and colors as the other buildings on the property. The privacy fence proposed along the south property line is proposed to be cedar wood, which is permanent, solid, and opaque. All condominium units have adequate space for storage of materials and vehicles, such these items do not need to be stored outside. Staff finds that this standard is met.

### **Building Form, Mass and Height**

Changes to the architecture of the buildings since the approval of the Preliminary Site Plan are generally summarized below:

- Minor changes to window shapes on Buildings A, B, and D
- Balcony exterior material changed from metal and tempered glass to wood railing and steel cords.
- Angled columns that supported balconies are now straight up and down.
- Garage-style balcony doors are predominately sliding glass doors now
- Building C was revised to include larger windows and additional rock, and a mid-level break with a lower roofline
- Roof pitch on all buildings, but particularly building D, has less slope due to truss design and assembly.

Standard 4.1.2 states, *“Buildings shall be designed to relate directly to and reinforce the pedestrian scale, the quality of the Blue River, and the street. The following techniques may be used to meet this objective:*

- a. *Shifts in building massing, variations in height, profile and roof form that provide human scale while maintaining a consistent relationship of the overall building form to the street edge and the river.*
- b. *Minimize long expanses of wall at a single height or in a single plane.*
- c. *Provide projecting elements; and*
- d. *Group elements to provide balanced façade composition.”*

Four building ‘types’ are proposed for the project. Each building type has similar design elements and building materials, resulting in a cohesive development. However, each building type also has distinct architecture, adding variety and visual interest to the project. All buildings have been designed to provide shifts in massing, variations in height, wall plane projections and recesses, and a variety of building materials. Staff finds that there is a balanced composition for each façade, and that this standard is met.

Standard 4.1.3 states, *“Reduce the bulk of a multi-story building to be on a pedestrian scale. The design shall emphasize a “base”, “middle”, and “top”:*

- a. *A distinctive base at the ground level shall use articulation and materials such as: timber, wood, stone, masonry, decorative concrete, or other materials. Base elements may include windows, architectural details, canopies, bays, or overhangs.*
- b. *The middle of the building shall be made distinct by change in material or color, windows, balconies, step-backs, and/or signage.*
- c. *The top of the building shall emphasize a distinct profile or outline with elements such as: a projecting parapet, cornice, upper level step-back, or creative roofline.”*

Each condominium building is proposed with a distinctive base, middle, and top. Large sections of stone are proposed for the base, attractively anchoring the building. The base is also emphasized with roof elements scaled to the pedestrian, windows, and entries. The middle of the building is made distinct by a change in material to cedar vertical and horizontal siding, angled timber columns, and balconies. The top of each building has a slightly sloped roof lines at varied elevations. Staff finds that this standard is met.

Standard 4.1.5 states, *“The mass of multi-family residential buildings shall be broken down to a human scale with creative window and door patterns, upper level balconies, angled wall planes, or other distinct architectural features.”* The mass of each of the condominium buildings is broken down to the human scale by attractive entries, projecting elements, timber columns, upper level balconies, ground level sloped roof features, covered entries, recessed garages, and large windows. This standard is met.

### ***Building Façades and Architectural Elements***

Standard 4.2.1 states, *“Buildings shall be designed to provide interest and variety, and with elements scaled to the pedestrian. The following techniques shall be used to meet this objective:*

- a. *Break up large building components with significant articulation of wall planes and roof lines;*
- b. *Create patterns, using window size and/or shape, that relate to interior functions;*
- c. *Emphasize building entries through projecting or recessed forms.*

Staff finds that the design of the five condominium buildings incorporates all of the elements required by this standard. There is significant articulation in the wall planes and roof lines, window patterns, a variety of building materials, and well defined entrances.

Standard 4.2.2 states, *“Provide human scale through change in plane, contrast, and intricacy in form. Avoid large areas of undifferentiated or blank building facades.”* Each of the condominium buildings is designed with a variety of high quality building materials, balconies, large windows, varied wall planes and rooflines, and timber columns, resulting in intricate form with human scaled elements. Staff finds that this standard is met.

Standard 4.2.5 states, *“Building facades shall not exceed 50 feet in length along the same geometric plane, at which time there shall be wall plane projections or recesses having a depth of at least 2 feet for a distance of not less than 6 feet.”* The condominium buildings meet this standard on each façade. To address a condition of approval of the Preliminary Site Plan, the applicant revised the garages to address this standard.

Standard 4.2.6 states, *“Each building façade shall have a repeating pattern that includes no less than 3 instances of either: color change, texture change, material change, or expression of a structural, architectural feature.”* Each of the types of condominium and garage buildings have at least 3 types of color, material, and texture change.

### ***Building Materials and Finishes***

Standard 4.3.3 states, *“Predominant exterior building materials shall be high-quality durable materials that retain their appearance over time, and that can be economically maintained. Buildings shall be predominantly clad in Class I and Class II materials. Class III materials are prohibited in the Riverfront District.”*

- *Class I materials include timber, log and wood siding, clay fired brick, natural stone, masonry, cement stucco, and glass.*
- *Class II materials include architectural metal, fiber cement siding, concrete brick, manufactured stone, and integrally colored split face block.*
- *Class III materials include EIFS, smooth-face concrete block, tilt-up concrete panel systems, metal panel systems, and vinyl and aluminum siding.*

The proposed building materials include timber columns, vertical and horizontal wood siding, manufactured stone, wood railing and steel cord balconies, metal roofing, and asphalt shingles. The materials are Class I and Class II materials, meeting this standard.

Standard 4.3.6 states, *“Applicants are required to submit a sample board of materials, finishes, and colors of all proposed exterior materials”.* The applicant has submitted a color copy of the material board. A color and material board was submitted with the Final

Site Plan and will be available at the public hearing.

### ***Building Colors***

Standard 4.4.2 states, “*The Munsell color notation system is broken into three characteristics: hue (color), chroma (brightness), and value (shade). In the Town of Silverthorne, chroma is the only Munsell color characteristic that is regulated.*”

- a. *The primary body colors of the building shall not exceed a chroma of 4 on the Munsell Color Chart.*
- b. *The trim accent colors of the building shall not exceed a chroma of 6. The trim accent colors are limited to an area of no greater than 10% of the building façade.*
- c. *The roof color of the building shall not exceed a chroma of 4. Roof color shall be compatible and complementary to the surrounding buildings in the Riverfront District.*
- d. *The use of black, white and neutral gray colors proposed for any portion of the building shall be reviewed based on the appropriateness to the proposed design.*

There are two color schemes for the condominium buildings. Types A, B, and D buildings have a lighter, more constrasty color scheme, while the Type C building and the garages has a darker and richer color scheme. Both color schemes meet the chroma requirements of this standard.

### ***Building Roofs***

Standard 4.5.2 states, “*Where pitched roofs are utilized, the use of gables, dormers, and shed roof elements shall be used to break up large expanses of roof, and to add architectural interest.*” The five condominium buildings are designed with a slightly pitched roofs. Roof lines are proposed at various angles and elevations, breaking up large expanses of roof. Pitched roof elements were added to the garage building to address this standard.

Standard 4.5.6 states, “*Visible roof surfaces shall be made of durable materials such as: concrete tile, metal, other pre-finished architectural metals or architectural grade asphalt shingles.*” Visible roof surfaces are proposed to be either asphalt shingles or standing seam metal, meeting this standard.

**STAFF RECOMMENDATION:** The Community Development Department recommends approval of the Minor Subdivision and Final Site Plan for Blue River Flats based on the following findings and with the following conditions:

#### ***Findings:***

1. The Minor Subdivision Plat meets all the requirements set forth in Section 4-5-11, Minor Subdivision, and Section 4-5-15, Certifications.
2. The Final Site Plan meets all the requirements set forth in Chapter 4, Article IV, Zoning Districts and Standards, and Article VI, Site Plan, with conditions.
3. The Minor Subdivision Plat and Final Site Plan meet the standards of the Riverfront Design District Standards and Guidelines.

**Conditions:**

1. That the Landscape Plan be modified to meet the required ratio of 50% coniferous trees and 50% deciduous trees, with the submittal of the building permit
2. That the Lighting Plan be revised such that all pole mounted light fixtures are on the property and setback at least as far as their heights from the property lines.
3. That a Real Estate Transfer Assessment document be submitted in conjunction with a Condominium Plat for the project, or prior to the transfer or sale of any condominium.
4. That the comments of the Town Engineer be addressed.
5. That the comments of the Utilities Manager be addressed prior to the start of site work or construction.

*Suggested Motion: "I move to recommend approval of the Minor Subdivision and Final Site Plan for Blue River Flats, with the Staff recommended conditions."*

**EXHIBITS:**

- Exhibit A: Project narrative
- Exhibit B: Minor Subdivision Plat
- Exhibit C: Final Site Plan set
- Exhibit D: Applicant response to referral agency comments
- Exhibit E: Utility Undergrounding Analysis, dated July 12, 2016
- Exhibit F: RETA proposal, dated July 19, 2016
- Exhibit G: Reasons for exempting first sale from RETA, dated September, 2016
- Exhibit H: Town Engineer Memo, dated September 28, 2016
- Exhibit I: Utilities Manager Memo, dated September 28, 2016



Godden|Sudik  
ARCHITECTS

PROJECT NARRATIVE: Blue River 50 - Silverthorne

Blue River 50 is 2.32 acres of existing developed land located on the west bank of the Blue River in the Riverfront District and zoned for 25 units per acre with a maximum Building Height of 35'-0" from average existing grade. The proposed development consists of 3 parcels, 1130, 1088 and 1056 Blue River Parkway. Approximately .5 acres are located within the Blue River and will remain undisturbed open space along with the required 25' river setback area. The river setback has been designed to both minimize environmental impacts and enhance the Blue River corridor by leaving approximately 45 existing, established trees, planting native trees and shrubs and including a 10'-0" paved path to connect pedestrians with adjacent properties.

Blue River 50 is designed as a 46 unit, for-sale Condominium Project offering a varied portfolio of home types. The project's units range from just under 900 square feet to over 1,800 square feet and were designed with large decks and windows to take advantage of the prominent location along the Blue River. 30 of the project's 46 units have direct or indirect views out to the Blue River as the buildings were sited intentionally along the river bends. In total, the development will consist of 6 separately built structures containing a total of 39 flat-stacked condominium residences, 5 townhome-style condominiums as well as 2 carriage house units with lofted living.

The development has 2 access points from Highway 9 to the west including one aligned along the properties northern boundary that provides shared access to the adjacent parcel. On site, there are 42 enclosed garage units (30 attached and 12 detached). There are 2 common access bike rooms and all 42 of the garage units are generously sized to fit bikes and the many recreation items enjoyed by Summit County residents. In addition to the garage spaces, there are 54 surface parking spaces supplying the required 96 spaces after a small bicycle parking reduction is applied.

To complement the scale of the surrounding neighborhoods, there are smaller, stand-alone two and one half-story residential buildings and townhome units interspersed with the three-story structures. These buildings along with the broken massing of the three-story buildings develop a scale appropriate for the location adjacent to State Highway 9 and the nearby larger commercial structures.

The archetypes established within the community will reflect the surrounding buildings and communities blending the historical as well as the contemporary. Low sloped roofs and generous overhangs help to create elongated horizontal planes of material and directly echo the strong mountain architecture established in the region. Natural stone provides a durable base and accents of stained natural wood give the buildings contextual flair.

The Blue River 50 Housing project will provide a much needed mix of for-sale housing to the Town of Silverthorne while respecting the river ecosystem and local character.

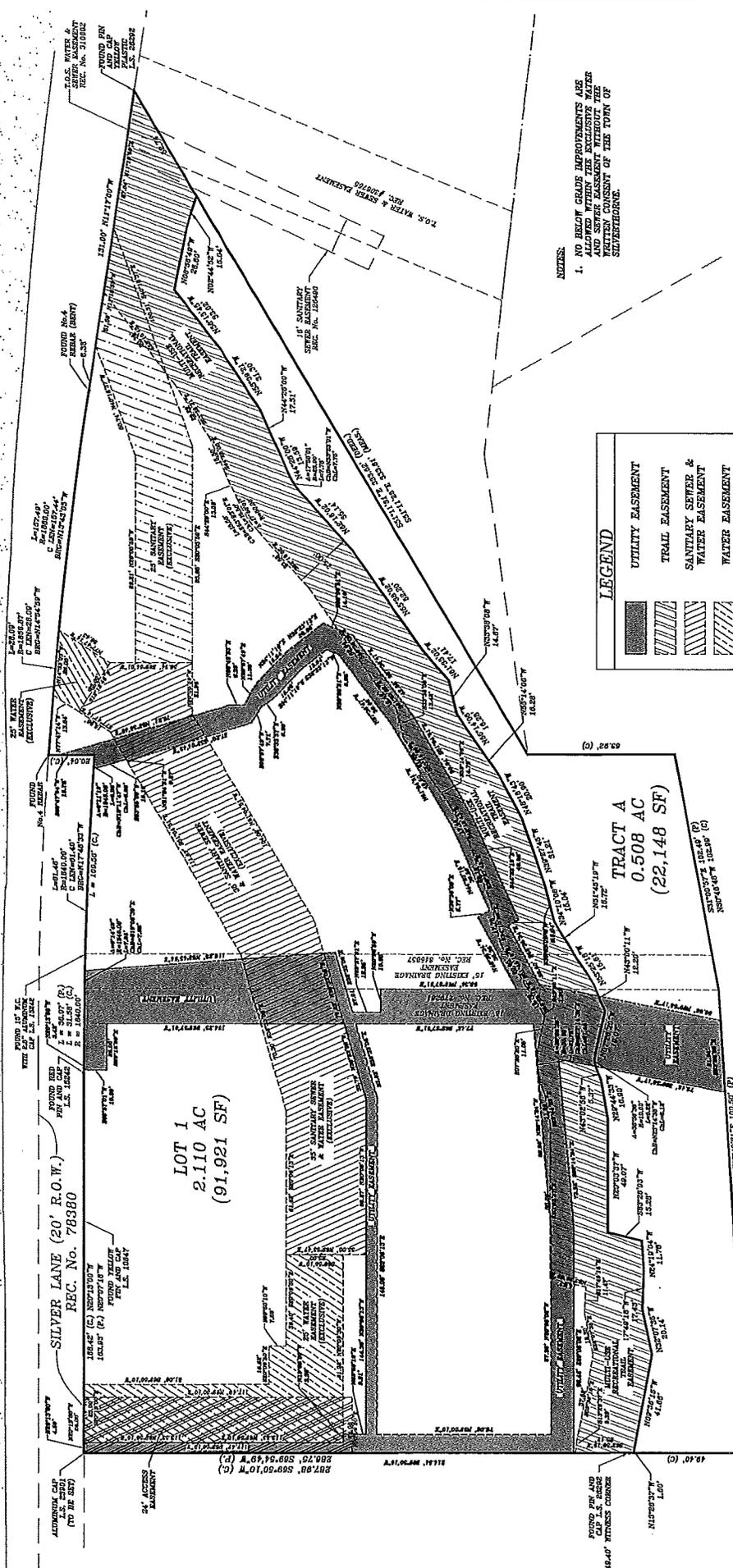
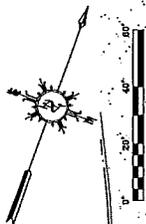


**BLUE RIVER FLATS CONDOMINIUMS**

PHASE 1 SUBDIVISION PLAT  
 LOTS 23R & 25, SILVERTHORNE SUBDIVISION  
 AND THE UNPLATTED 0.704 AC. TRACT RECORDED AT REC. NO. 633874  
 LOCATED IN THE N 1/2 OF SEC. 1, T.5 S., R.78 W. OF 6th P.M.

TOWN OF SILVERTHORNE  
 SUMMIT COUNTY, COLORADO  
 SHEET 2 OF 2

BLUE RIVER PARKWAY  
 COLORADO STATE HIGHWAY No. 9



NOTES:  
 1. NO BELOW GRADE IMPROVEMENTS ARE ALLOWED WITHIN THE EXCLUSIVE WATER RIGHTS EASEMENT UNLESS WITH THE WRITTEN CONSENT OF THE TOWN OF SILVERTHORNE.

**LEGEND**

- UTILITY EASEMENT
- TRAIL EASEMENT
- SANITARY SEWER & WATER EASEMENT
- WATER EASEMENT
- ACCESS EASEMENT
- SANITARY SEWER EASEMENT

Baseline Surveys LLC		PLANNED BY TOWN OF SILVERTHORNE
DATE: 2/4/2016		SCALE: 1" = 20'
DRAWN BY: K.F.O.		CHECKED BY: D.S.O.
JOB FILE: 4083		DWG FILE: 4083.P2
DATE: 2/4/2016		SHEET 2 OF 2

NOTES: ACCORDING TO COLORADO LAW, YOU MUST EXAMINE ANY UTILIZATION BASED ON THE DATE OF RECORDATION OF THIS PLAT. ANY UTILIZATION BASED ON THIS DATE OF RECORDATION IS NOT VALID. ANY UTILIZATION BASED ON THIS DATE OF RECORDATION IS NOT VALID. ANY UTILIZATION BASED ON THIS DATE OF RECORDATION IS NOT VALID.





NOT FOR CONSTRUCTION



Godden Sjudik ARCHITECTS  
1000 West 12th Street  
Suite 1000  
Denver, Colorado 80202  
Tel: 303.733.1234  
Fax: 303.733.1235  
www.goddensjudik.com

# Blue River Flats

1056, 1088, and 1130 Blue River Parkway Silverthorne, CO

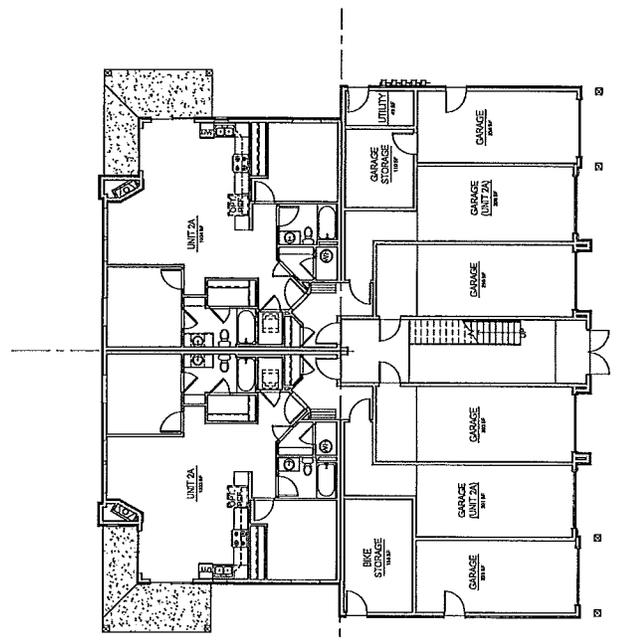
FINAL SITE APPLICATION  
DRAWN BY:  
CHECKED BY:  
ISSUE DATE:  
07-29-2016

REVISIONS	DATE	DESCRIPTION

BUILDING PLANS

# A1.50

© Copyright 2015  
Godden Sjudik Architects, P.C.







NOT FOR  
CONSTRUCTION



1000 GARDEN CITY AVENUE  
SUITE 1000  
GARDEN CITY, NY 11530  
TEL: 516.466.1000  
WWW.GODDENSTUDIK.COM

Godden | Studik  
ARCHITECTS

# Blue River Flats

1056, 1088, and 1130 Blue River  
Parkway Silverthorne, CO

FINAL SITE  
APPLICATION

DRAWN BY:  
CHECKED BY:  
ISSUE DATE:  
07-29-2018

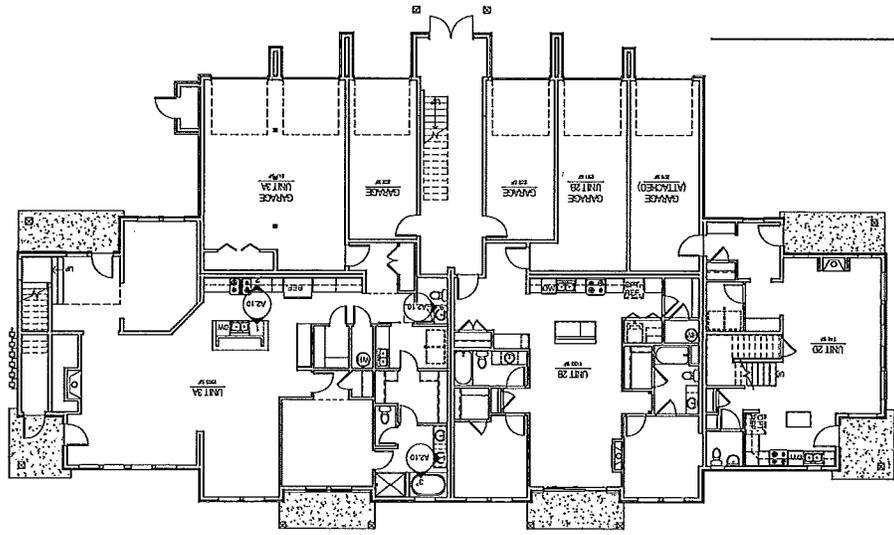
REVISIONS		

BUILDING D PLANS

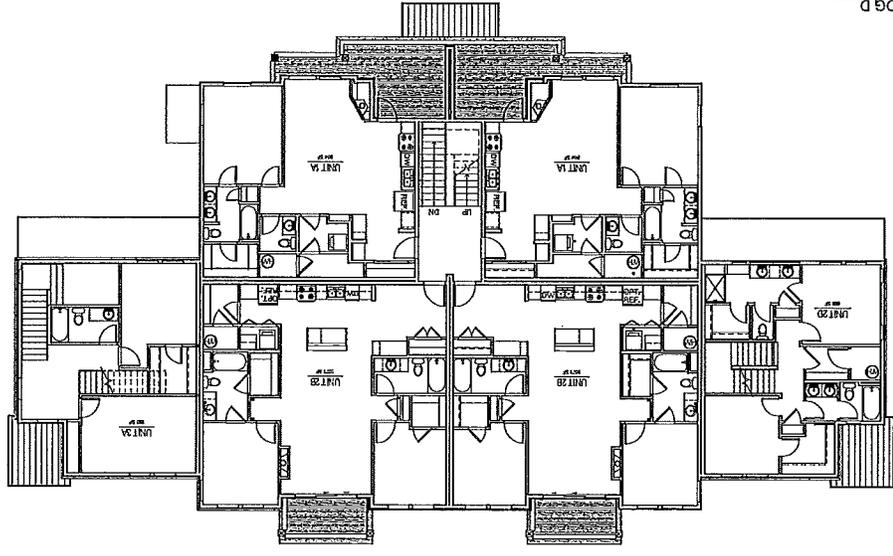
# A1.53

© Copyright 2018  
Godden | Studik Architects & Planners, Inc.

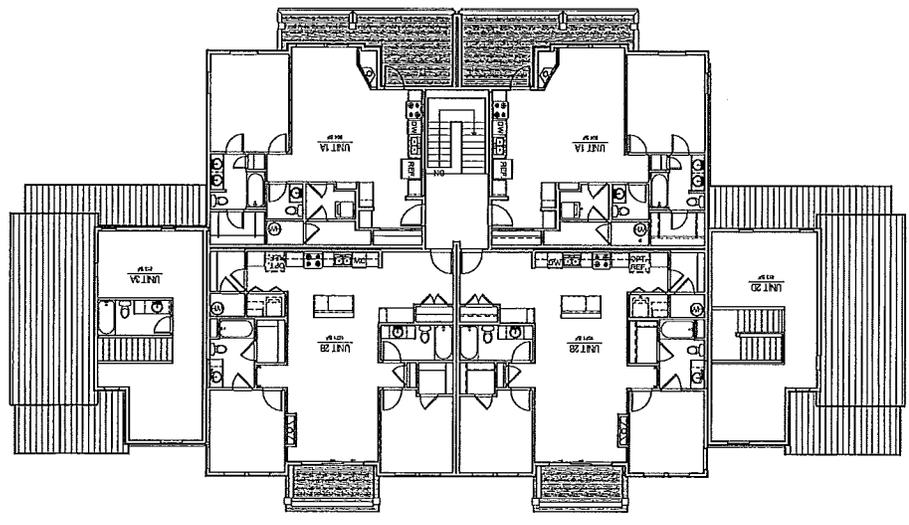
1 1ST LEVEL - BLDG D  
1/8" = 1'-0"



2 2ND LEVEL - BLDG D  
1/8" = 1'-0"



3 3RD LEVEL - BLDG D  
1/8" = 1'-0"





NOT FOR CONSTRUCTION



Godden | Sudik  
ARCHITECTS

**Blue River Flats**  
1056, 1088, and 1130 Blue River  
Parkway, Silverthorne, CO

FINAL SITE APPLICATION	
DRAWN BY:	
CHECKED BY:	
ISSUE DATE:	07-20-2016
REVISIONS	

BUILDING A  
ELEVATIONS

**A3.01**

© 2016 GODDEN | SUDIK ARCHITECTS, P.C.

BLUE RIVER FLATS - SILVERTHORNE FINAL PLAN APPLICATION SUBMITTAL

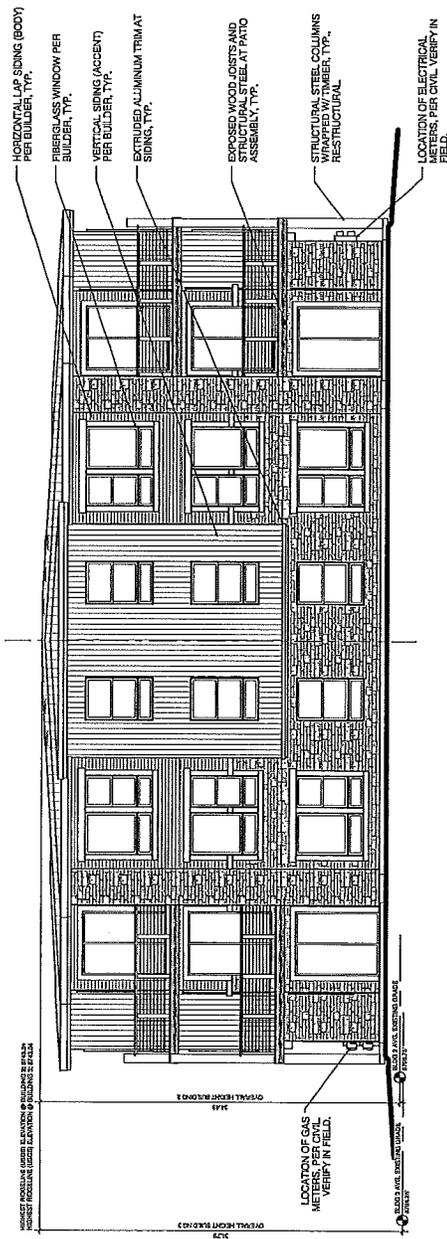
NO BUILDING SHALL EXCEED THE MAXIMUM BUILDING HEIGHT OF 35'4" AS PER THE SILVERTHORNE OVERSIGHT DISTRICT DESIGN GUIDELINES.

WHERE POSSIBLE, ROOF PENETRATIONS SHALL BE GROUPED TOGETHER, LOCATED ON REAR ROOF SLOPES AND SHALL NOT BE LOCATED ON FRONT SLOPES. ROOF PENETRATIONS SHALL BE PAINTED TO MATCH THE ROOF MATERIAL.

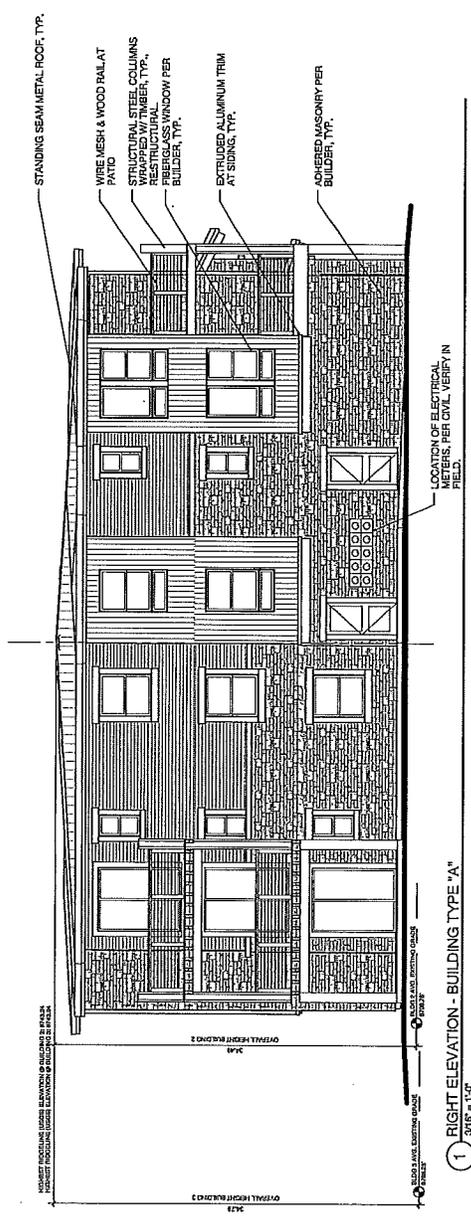
ROOF TO GUTTER SHALL BE LOW PROFILE AND SHALL BE PAINTED TO MATCH ROOF MATERIAL.

ELECTRICAL AND GAS METERS ARE SHOWN ON BUILDING ELEVATIONS. PLEASE REFER TO UTILITY PLAN FOR COORDINATION.

PLEASE REFER TO GRADING PLAN FOR LOT SPECIFIC GRADING.



2 REAR ELEVATION - BUILDING TYPE "A"  
3/16 - 1/4"



1 RIGHT ELEVATION - BUILDING TYPE "A"  
3/16 - 1/4"

NOT FOR CONSTRUCTION



Godden | Sudik  
ARCHITECTS

# Blue River Flats

1056, 1088, and 1130 Blue River Parkway Silverthorne, CO

FINAL SITE APPLICATION	REVISIONS
DRAWN BY:	
CHECKED BY:	
ISSUE DATE:	
07-26-2016	

BUILDINGS ELEVATIONS

**A3.02**

**BLUE RIVER FLATS - SILVERTHORNE FINAL PLAN APPLICATION SUBMITTAL**

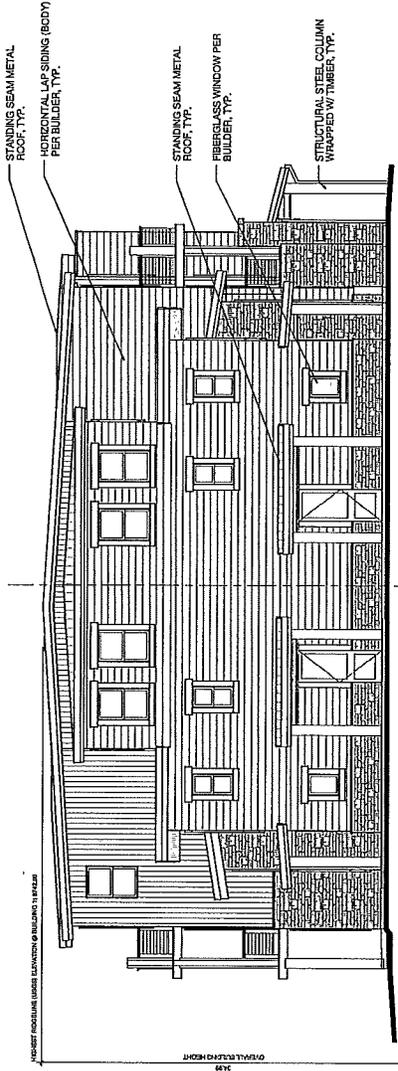
NO BUILDING SHALL EXCEED THE MAXIMUM BUILDING HEIGHT OF 34 FEET AS SHOWN ON THE SILVERTHORNE RIVERFRONT DISTRICT DESIGN GUIDELINES.

WHERE POSSIBLE, ROOF PENETRATIONS SHALL BE SECURED TOGETHER, LOCATED ON REAR ROOF SLOPES AND SHALL NOT BREACH RIDGELINES. ALL ROOF PENETRATIONS SHALL BE PAINTED TO MATCH THE ROOF MATERIAL.

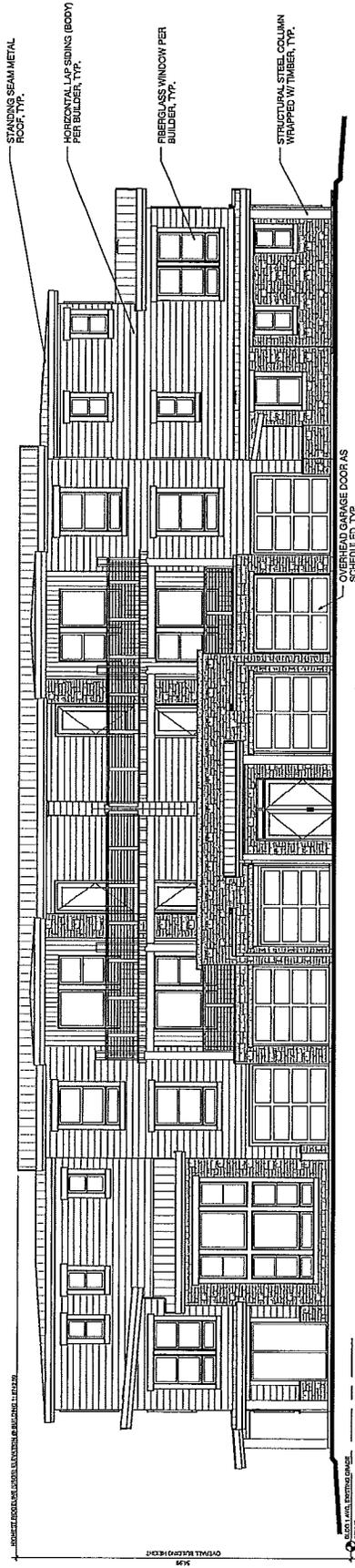
ROOF/ATTIC VENTS SHALL BE LOW-PROFILE AND SHALL BE PAINTED TO MATCH ROOF MATERIAL.

ELECTRICAL AND GAS METERS ARE SHOWN ON BUILDING ELEVATIONS. PLEASE REFER TO UTILITY PLAN FOR OCCUPANCY.

PLEASE REFER TO GRADING PLAN FOR LOT SPECIFIC GRADING.



2 LEFT ELEVATION - BUILDING TYPE "B"  
3/16" = 1'-0"



1 FRONT ELEVATION - BUILDING TYPE "B"  
3/16" = 1'-0"

NOT FOR CONSTRUCTION



City of Silverthorne  
 Planning & Development  
 1000 West 104th Avenue  
 Suite 200  
 Silverthorne, CO 80483  
 Phone: 303.441.2000  
 Fax: 303.441.2001  
 www.silverthorne.com

**Blue River Flats**  
 1056, 1088, and 1130 Blue River  
 Parkway, Silverthorne, CO

FINAL SITE APPLICATION	REVISIONS
DRAWN BY:	
CHECKED BY:	
ISSUE DATE:	
07-29-2018	

BUILDING & ELEVATIONS

**A3.03**

BLUE RIVER FLATS - SILVERTHORNE FINAL PLAN APPLICATION SUBMITTAL

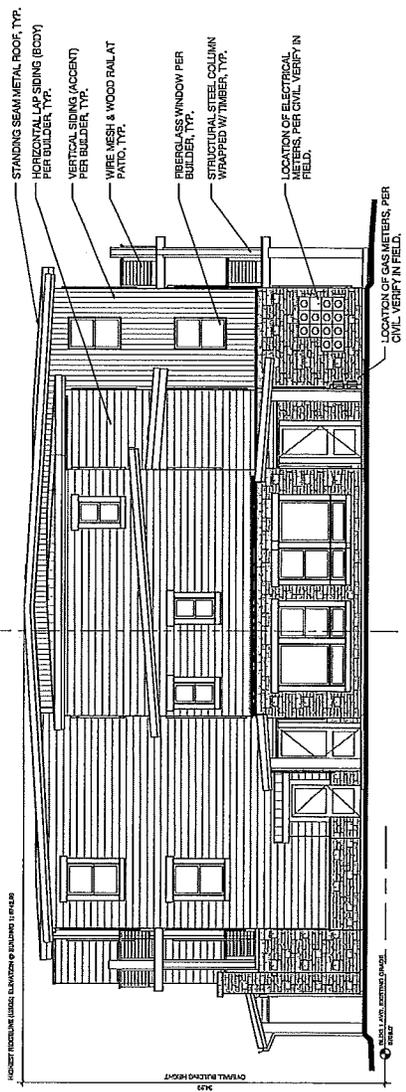
NO BUILDING SHALL EXCEED THE MAXIMUM BUILDING HEIGHT OF 28' AS PER THE SILVERTHORNE INTERTOWN DISTRICT DESIGN GUIDELINES.

WHERE POSSIBLE, ROOF PENETRATIONS SHALL BE GROUPED TOGETHER, LOCATED ON REAR ROOF SLOPES AND SHALL NOT BREACH RIDGELINES. ALL ROOF PENETRATIONS SHALL BE PAINTED TO MATCH THE ROOF MATERIAL.

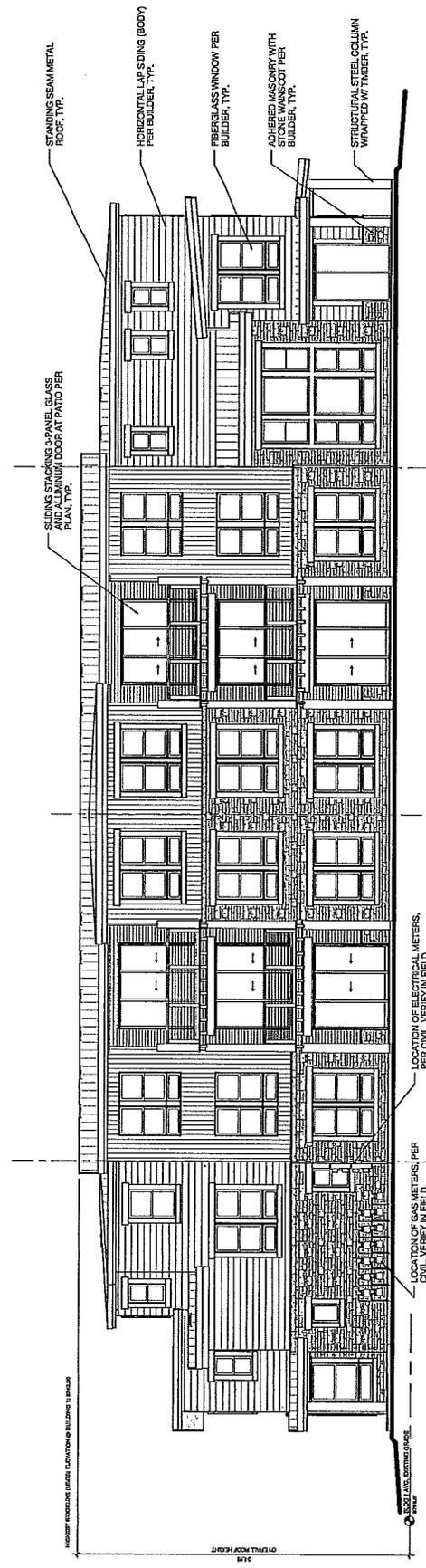
ROOF/ATTIC VENTS SHALL BE LOW-PROFILE AND SHALL BE PAINTED TO MATCH ROOF MATERIAL.

ELECTRICAL AND GAS METERS ARE SHOWN ON BUILDING ELEVATIONS. PLEASE REFER TO UTILITY PLAN FOR COORDINATION.

PLEASE REFER TO GRADING PLAN FOR LOT SPECIFIC GRADING.



2 RIGHT ELEVATION - BUILDING TYPE "B"  
 3/16" = 1'-0"



1 REAR ELEVATION - BUILDING TYPE "B"  
 3/16" = 1'-0"

NOT FOR CONSTRUCTION



PLANNING DEPARTMENT  
SILVERTHORNE, CO  
1056, 1088, and 1130 Blue River Parkway, Silverthorne, CO

Godden | Sudik ARCHITECTS

# Blue River Flats

FINAL SITE APPLICATION
DRAWN BY:
CHECKED BY:
ISSUE DATE: 07/20/2018
REVISIONS

BUILDING C ELEVATIONS

**A3.04**

© Copyright 2018  
Godden | Sudik Architects P.C.

BLUE RIVER FLATS - SILVERTHORNE FINAL PLAN APPLICATION SUBMITTAL

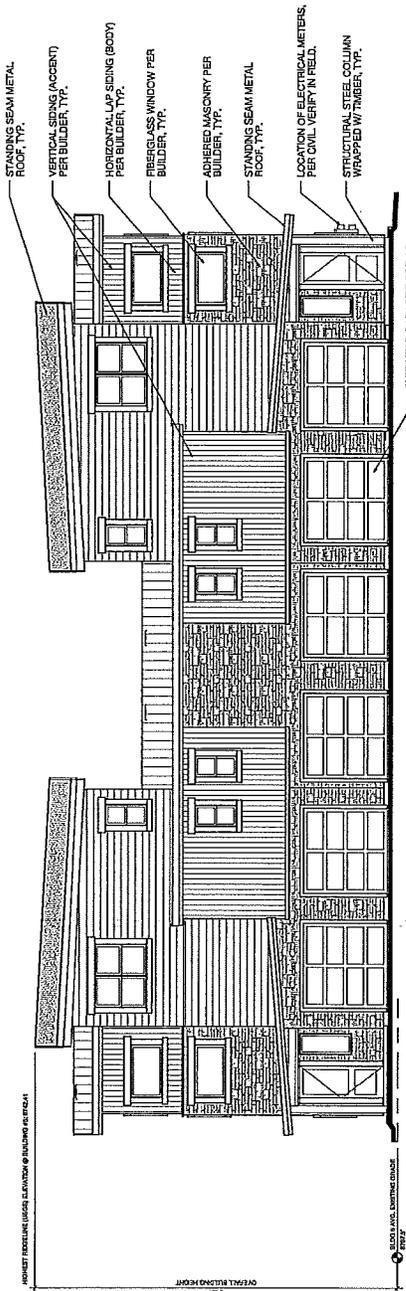
NO BUILDING SHALL EXCEED THE MAXIMUM BUILDING HEIGHT OF 35 FEET. THE SILVERTHORNE HEIGHT DISTRICT DESIGN GUIDELINES.

WHERE POSSIBLE, ROOF PENETRATIONS SHALL BE GROUPED TOGETHER, LOCATED ON REAR ROOF SLOPES AND SHALL NOT BREACH RIDGELINES. ALL ROOF PENETRATIONS SHALL BE PAINTED TO MATCH THE ROOF MATERIAL.

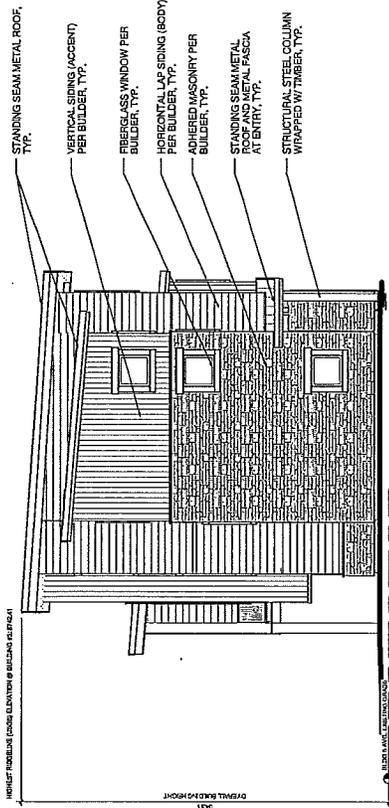
ROOF/ATTIC VENTS SHALL BE LOW-PROFILE AND SHALL BE PAINTED TO MATCH ROOF MATERIAL.

ELECTRICAL AND GAS METERS ARE SHOWN ON BUILDING ELEVATIONS. PLEASE REFER TO UTILITY PLAN FOR COORDINATION.

PLEASE REFER TO GRADING PLAN FOR LOT SPECIFIC GRADING.



2 FRONT ELEVATION - BUILDING TYPE 'C'  
3/16" = 1'-0"



1 LEFT ELEVATION - BUILDING TYPE 'C'  
3/16" = 1'-0"



NOT FOR CONSTRUCTION



Blue River Flats  
1056, 1088, and 1100 Blue River Parkway  
Silverthorne, CO 80421

Godden | Studik  
ARCHITECTS

# Blue River Flats

1056, 1088, and 1100 Blue River Parkway, Silverthorne, CO

FINAL SITE APPLICATION	
DRAWN BY:	
CHECKED BY:	
ISSUE DATE:	07-28-2018
REVISIONS	

BUILDING D  
ELEVATIONS

**A3.06**

© 2018 Godden | Studik Architects, Inc.

BLUE RIVER FLATS - SILVERTHORNE FINAL PLAN APPLICATION SUBMITTAL

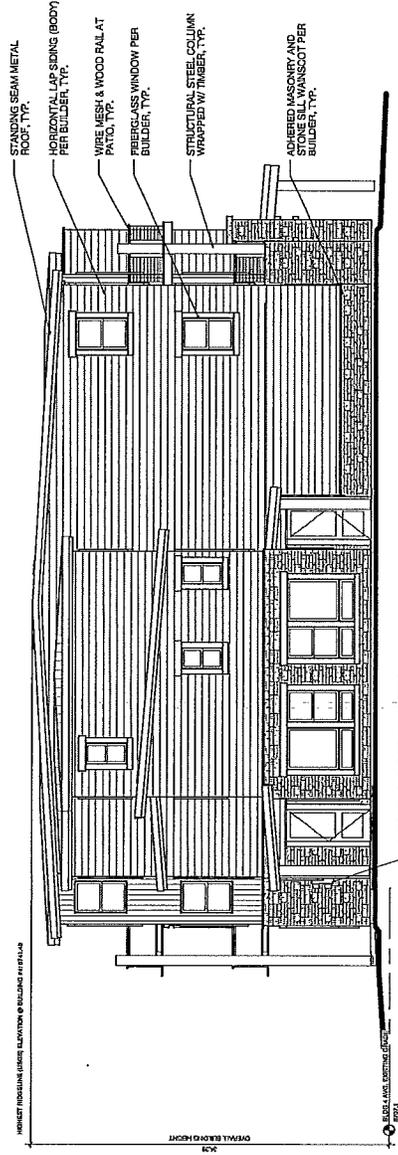
NO BUILDING SHALL EXCEED THE MAXIMUM BUILDING HEIGHT OF 35' AS PER THE SILVERTHORNE RIVERFRONT DISTRICT DESIGN GUIDELINES.

WHERE POSSIBLE, ROOF PENETRATIONS SHALL BE GROUPED TOGETHER, LOCATED ON REAR ROOF SLOPES AND SHALL NOT BE LOCATED ON FRONT ROOF SLOPES. ROOF PENETRATIONS SHALL BE PAINTED TO MATCH THE ROOF MATERIAL.

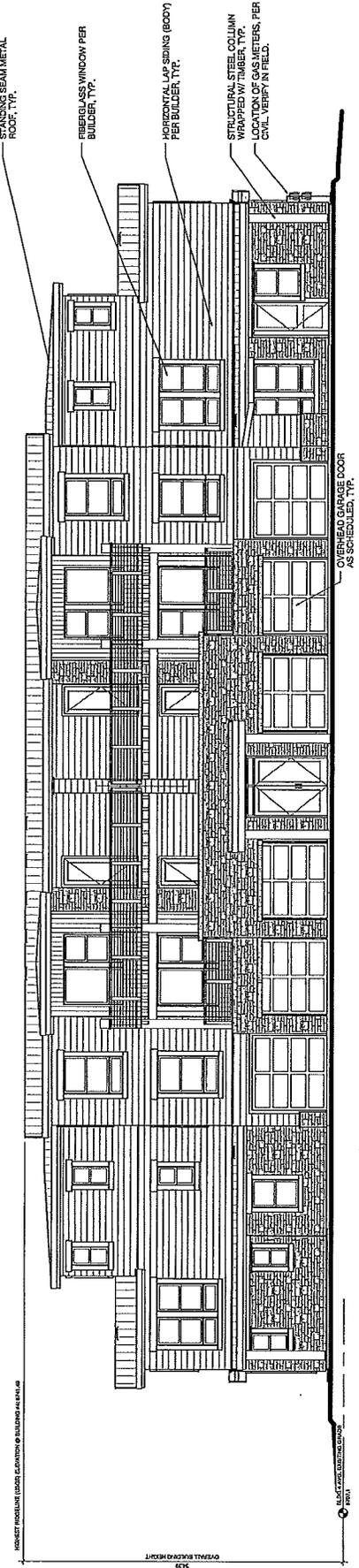
ROOF FLASHING SHALL BE LOW-PROFILE AND SHALL BE PAINTED TO MATCH ROOF MATERIAL.

ELECTRICALS AND GAS METERS ARE SHOWN ON BUILDING ELEVATIONS. SEE PERMIT TO UTILITY PART 151 FOR COORDINATION.

PLEASE REFER TO GRADING PLAN FOR LOT SPECIFIC GRADING.



2 LEFT ELEVATION - BUILDING TYPE "D"  
3/16" = 1'-0"



1 FRONT ELEVATION - BUILDING TYPE "D"  
3/16" = 1'-0"

NOT FOR CONSTRUCTION



1059, 1088, and 1130 Blue River Parkway Silverthorne, CO  
 800.441.2222  
 303.441.2222  
 303.441.2222  
 303.441.2222

Godden | Sudik ARCHITECTS

# Blue River Flats

1059, 1088, and 1130 Blue River Parkway Silverthorne, CO

FINAL SITE APPLICATION

DRAWN BY:

CHECKED BY:

ISSUE DATE: 07-29-2016

REVISIONS

NO.	DESCRIPTION

BUILDING ELEVATIONS

**A3.07**

© Copyright 2016  
 Silverthorne Construction, Inc.

**BLUE RIVER FLATS - SILVERTHORNE FINAL PLAN APPLICATION SUBMITTAL**

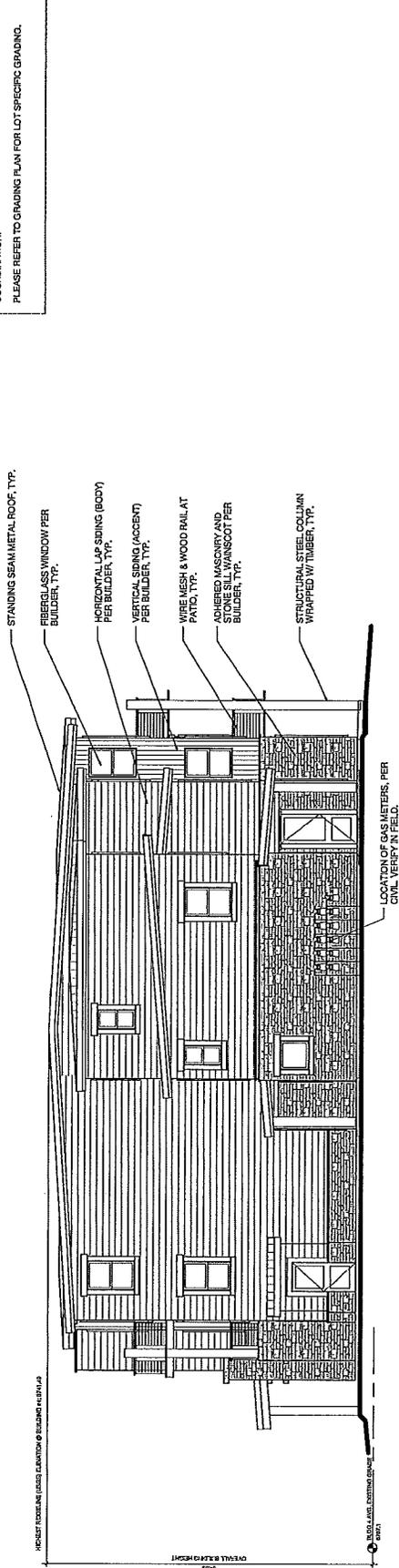
NO BUILDING SHALL EXCEED THE MAXIMUM BUILDING HEIGHT OF 35 FEET AT THE SILVERTHORNE NEIGHBORHOOD DISTRICT DESIGN GUIDELINES.

WHERE POSSIBLE, ROOF PENETRATIONS SHALL BE COUPLED TOGETHER, LOCATED ON REAR ROOF SLOPES AND SHALL NOT BREACH RIDGELINES. ALL ROOF PENETRATIONS SHALL BE PAINTED TO MATCH THE ROOF MATERIAL.

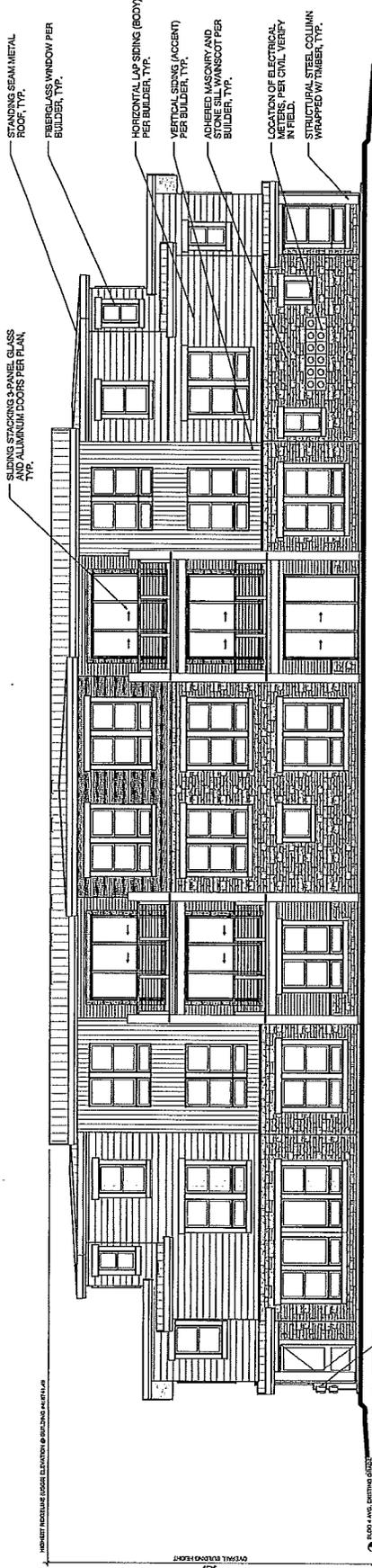
ROOF/ATTIC VENTS SHALL BE LOW-PROFILE AND SHALL BE PAINTED TO MATCH ROOF MATERIAL.

ELECTRICAL AND GAS METERS ARE SHOWN ON BUILDING ELEVATIONS. METERS SHALL BE LOCATED IN FIELD. PLEASE REFER TO UTILITY PLAN FOR COORDINATION.

PLEASE REFER TO GRADING PLAN FOR LOT SPECIFIC GRADING.



2 RIGHT ELEVATION - BUILDING TYPE "D"  
 3/16" = 1'-0"



1 RIVER ELEVATION - BUILDING TYPE "D"  
 3/16" = 1'-0"



**NOT FOR CONSTRUCTION**



1056, 1088, AND 130 BLUE RIVER PARKWAY SILVERTHORNE, CO  
 800.441.1111  
 WWW.BLUERIVERFLATS.COM  
 Blue River Flats  
 1056, 1088, AND 130 BLUE RIVER PARKWAY SILVERTHORNE, CO 800.441.1111  
 WWW.BLUERIVERFLATS.COM

Godden | Sudik  
 ARCHITECTS

**Blue River Flats**  
 1056, 1088, and 130 Blue River Parkway, Silverthorne, CO

FINAL SITE APPLICATION

DRAWN BY:

CHECKED BY:

ISSUE DATE:

07-20-2016

REVISIONS

NO.	DESCRIPTION

GARAGE ELEVATIONS

**A3.09**

© Copyright 2016  
 Godden | Sudik Architects & Planners, Inc.

**BLUE RIVER FLATS - SILVERTHORNE FINAL PLAN APPLICATION SUBMITTAL**

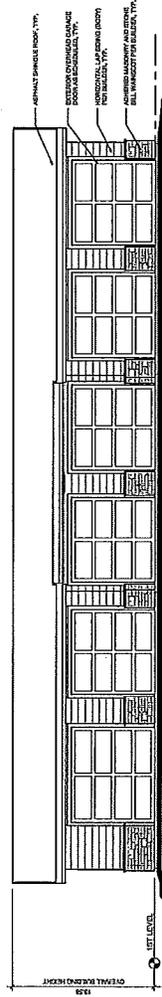
NO BUILDING SHALL EXCEED THE MAXIMUM BUILDING HEIGHT OF 15.00 FEET AT THE SILVERTHORNE RIVERFRONT DISTRICT DESIGN GUIDELINES.

WHERE POSSIBLE, ROOF PENETRATIONS SHALL BE GROUPED TOGETHER, LOCATED ON REAR ROOF SLOPES AND SHALL NOT BREACH RIDGELINES. ALL ROOF PENETRATIONS SHALL BE PAINTED TO MATCH THE ROOF MATERIAL.

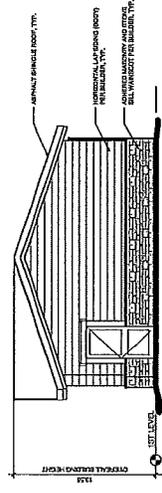
ROOF / ATTIC VENTS SHALL BE LOW-PROFILE AND SHALL BE PAINTED TO MATCH ROOF MATERIAL.

ELECTRICAL AND GAS METERS ARE SHOWN ON BUILDING ELEVATIONS. PLEASE REFER TO UTILITY PLAN FOR COORDINATION.

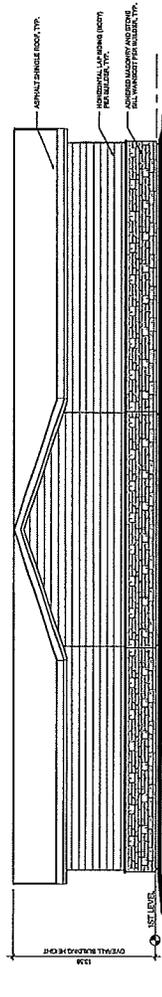
PLEASE REFER TO GRADING PLAN FOR LOT SPECIFIC GRADING.



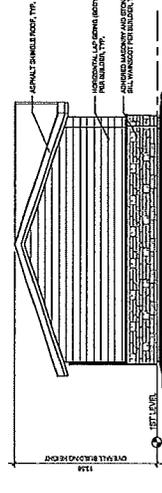
4 FRONT ELEVATION - GARAGE  
 3/16" = 1'-0"



3 LEFT ELEVATION - GARAGE  
 3/16" = 1'-0"

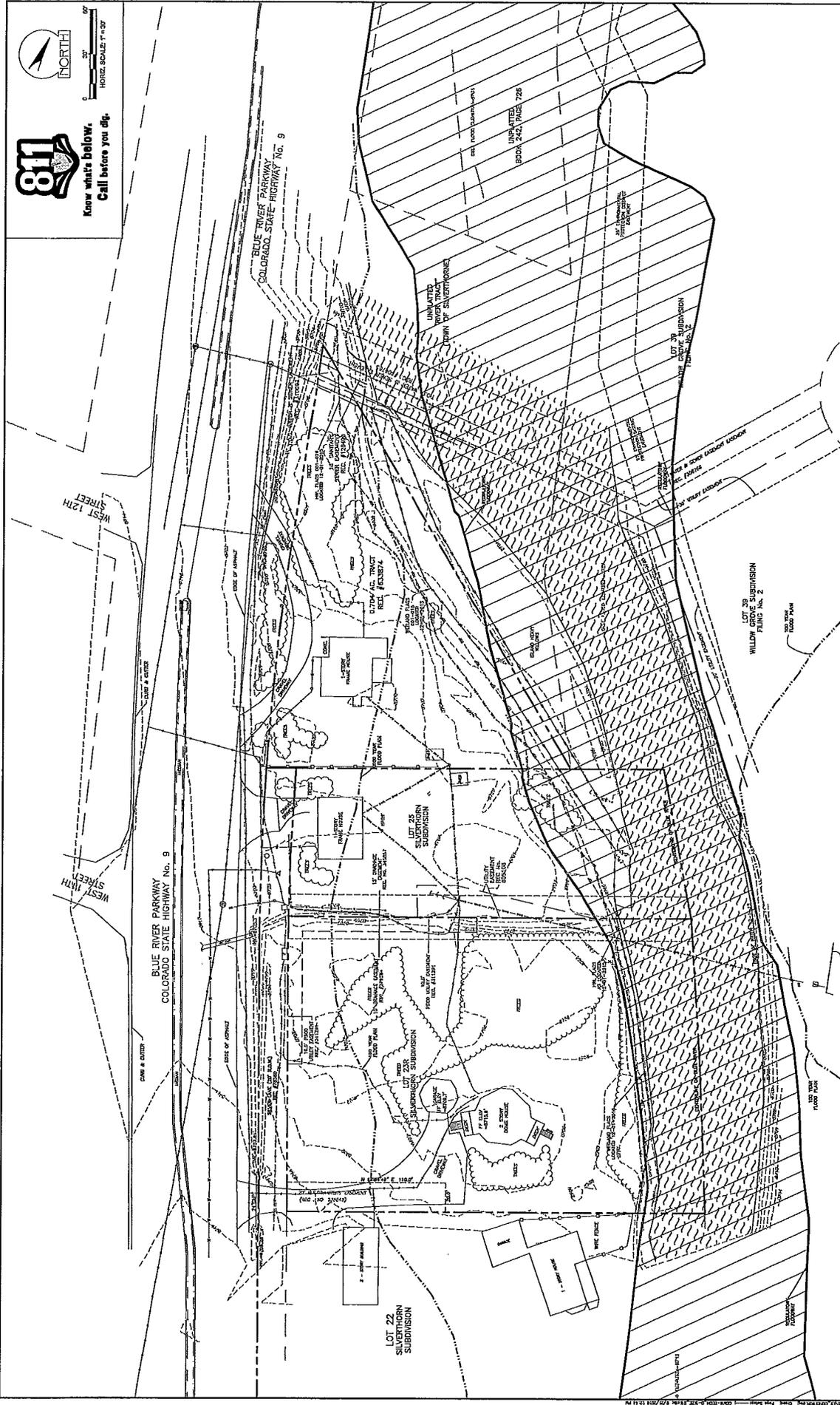


2 REAR ELEVATION - GARAGE  
 3/16" = 1'-0"



1 RIGHT ELEVATION - GARAGE  
 3/16" = 1'-0"





**811**  
 Know what's below.  
 Call before you dig.

**NORTH**  
 0 30' 60'  
 HORIZONTAL SCALE: 1" = 30'

DATE SEPT. 2016		SHEET C2	
<b>BLUE RIVER FLATS</b> EXISTING CONDITIONS MAP			
<b>ICON ENGINEERING, INC.</b> 1000 W. 10TH AVENUE, SUITE 100 DENVER, COLORADO 80202 303.733.8888			
<b>SUBDIVISION COLORADO</b>			
PLANNING	DESIGNED	DRAWN	CHECKED
RESEARCHED	MADE	CHECKED	TWC
UTILITIES	RESEARCHED	DRAWN	CHECKED
APPR.	DATE	REVISION	

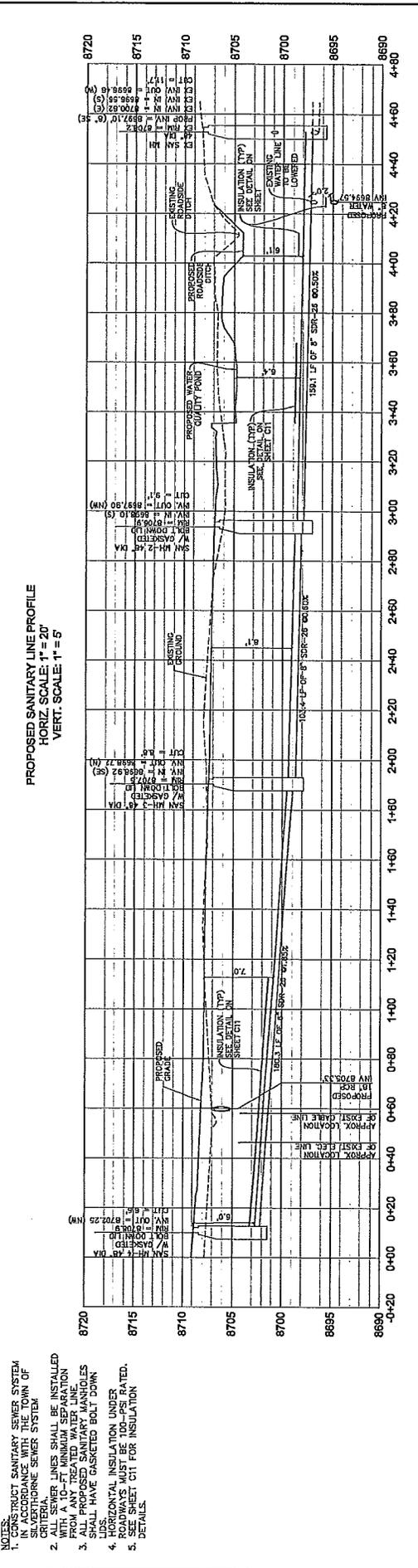
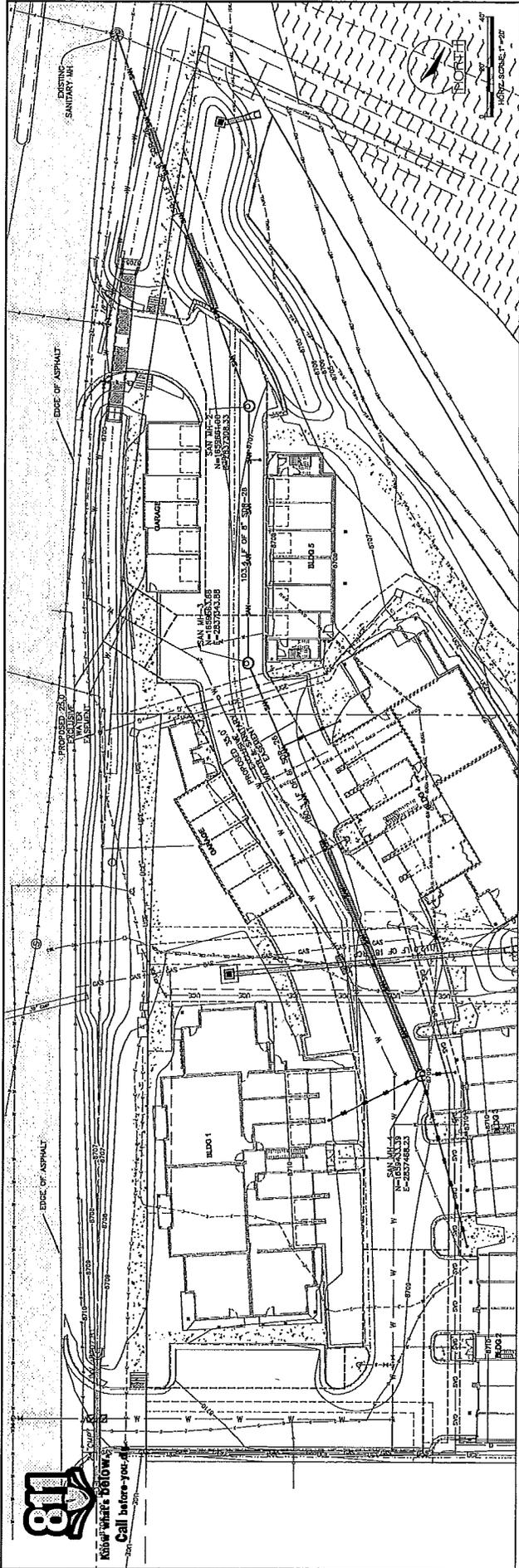












**ICON ENGINEERING INC.**  
 SILVERTHORNE COLORADO

**BLUE RIVER FLATS**  
 SANITARY SEWER PLAN & PROFILE

DATE: SEPT 2016  
 SHEET: C8

ICON PROJECT NO. 15-005-SE-352

NO.	DATE	REVISIONS

PLAN	UTILITIES
DRAWN	RESEARCHED
DESIGNED	DRAWN
CHECKED	CHECKED
TWC	TWC

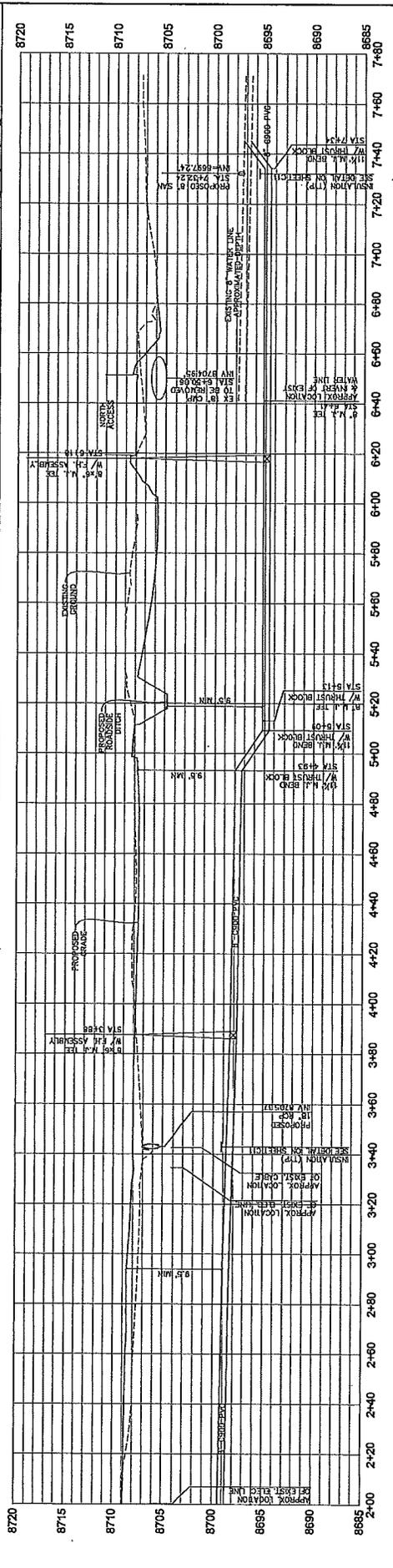
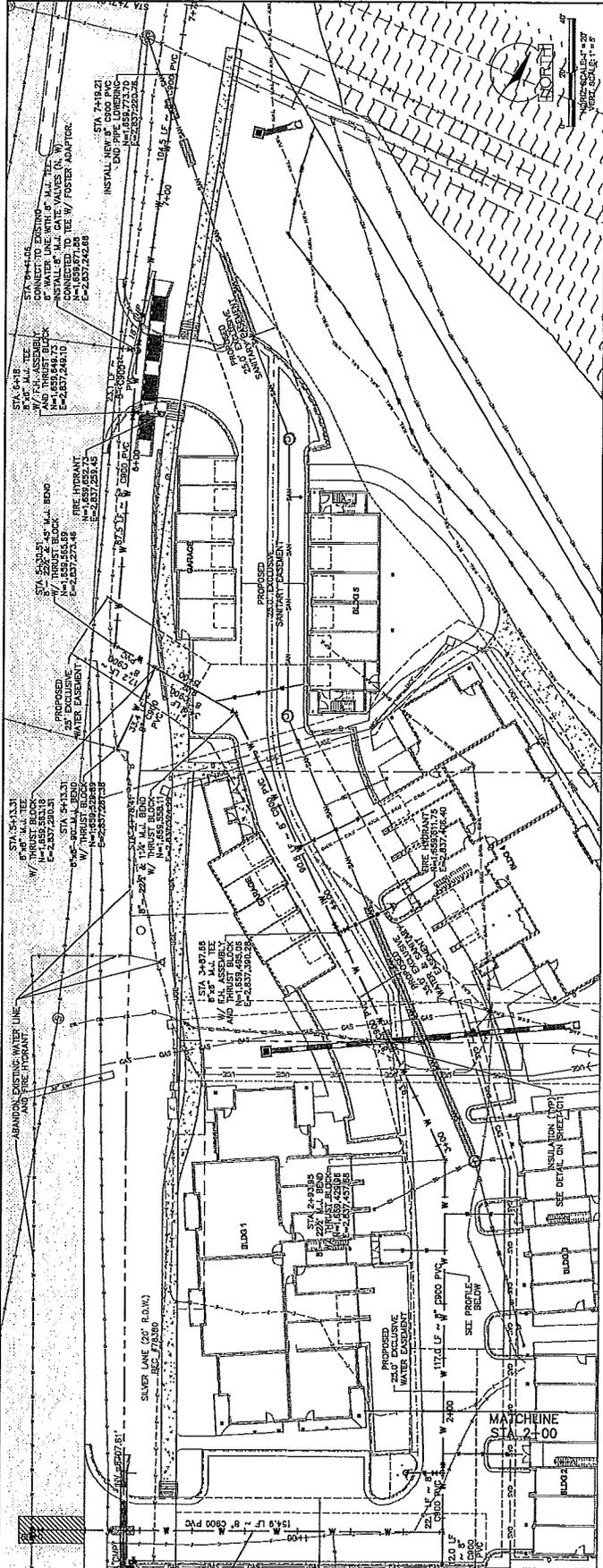
APPR. \_\_\_\_\_

- NOTES:**
1. EXISTING SANITARY SEWER SYSTEM TO BE REMOVED AND REPLACED WITH SILVERTHORNE SEWER SYSTEM.
  2. CRITERIA LINES SHALL BE INSTALLED WITH A 10'-FT MINIMUM SEPARATION FROM ANY TREATED WATER LINE.
  3. ALL PROPOSED SANITARY MANHOLES LIDS SHALL HAVE GASKETED BOLT DOWN LIDS.
  4. HORIZONTAL INSULATION UNDER GROUND SHALL BE 100-PSI RATED, ROADWAYS MUST BE 100-PSI RATED, SEE SHEET C11 FOR INSULATION DETAILS.





Know what's below  
 Call before you dig



NO.	DATE	REVISIONS	APPR.	TWC	CHECKED	DRAWN	DESIGNED	MASS	RESEARCHED	UTILITIES

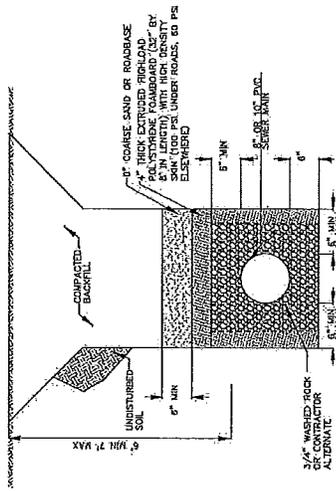


**ICON ENGINEERING INC.**  
 1000 W. 100th Street, Suite 100  
 Golden, CO 80401

**BLUE RIVER FLATS**  
**WATER PLAN AND PROFILE 2**

DATE: **SEPT 2018**  
 SHEET: **C10**

CONTRACT PROJECT NO. **18-000-HE-358**



**TYPICAL INSULATION DETAIL**

**TYPICAL INSULATION DETAIL**

SCALE: N.T.S.

**WATER NOTES:**

1. ALL WATER MAIN SHALL BE 8-IN MINIMUM DIAMETER AND C900 PVC.
2. ALL WATER MAIN SHALL BE INSTALLED WITH CONDUCTIVITY CONNECTIONS.
3. ALL WATER MAIN SHALL BE INSTALLED WITH A 9.5-FT MINIMUM AND 12-FT MAXIMUM COVER FROM TOP OF PIPE.
4. WATERLINES WITH 8-FT TO 9.5-FT COVER SHALL BE INSULATED WITH 4-IN BLUEBOARD INSULATING MATERIAL.
5. WATERLINES LESS THAN 8 FT OF COVER SHALL INSULATE ON THREE SIDES (TOP AND BOTH SIDES) WITH 4-IN BLUEBOARD. SIDE INSULATION SHALL HAVE SPACERS RATS.
6. HORIZONTAL INSULATION UNDER ROADWAYS MUST BE 100-PSI RATED.
7. ALL WATER SERVICES SHALL BE INSULATED UNDER ALL DRIVEWAYS, ROADWAYS, OR PARKING AREAS OR WHERE ANY PART OF THE SERVICE LINE IS LESS THAN 9.5-FT BELOW GROUND SURFACE.

**SANITARY SEWER NOTES:**

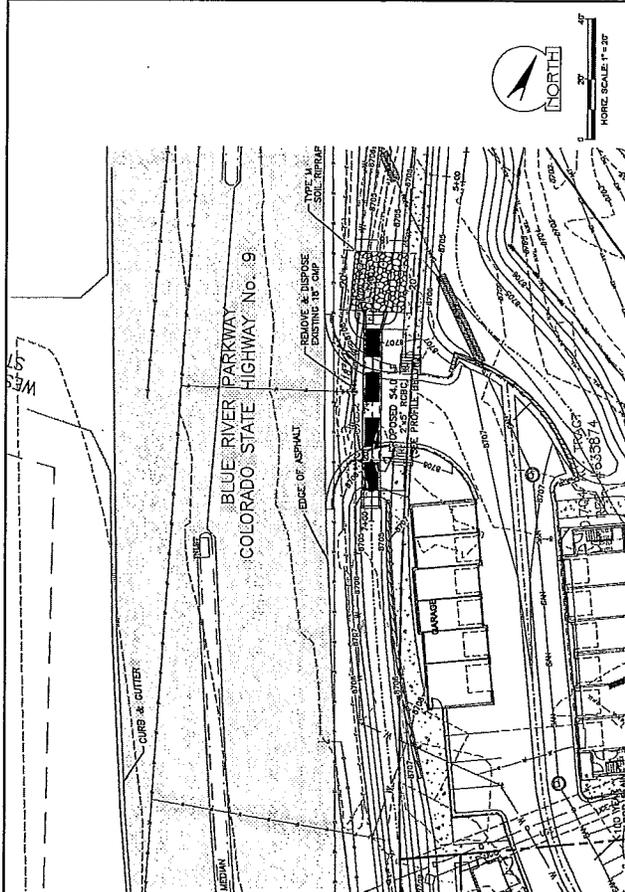
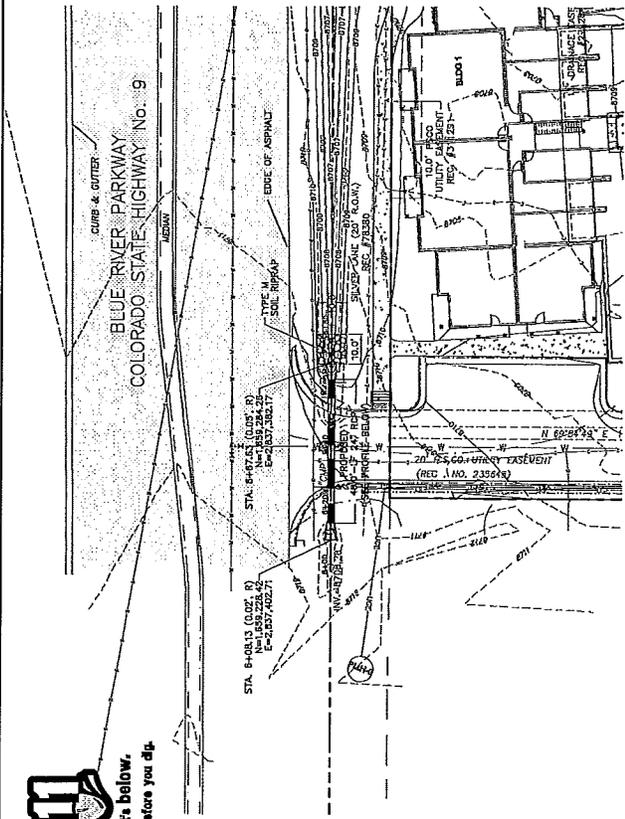
1. ALL SANITARY SEWER MAIN SHALL BE 8-IN MINIMUM DIAMETER AND SDR-35.
2. ALL SANITARY SEWER MAIN SHALL HAVE 12-FT MINIMUM COVER FROM TOP OF PIPE.
3. SANITARY SEWER WITH 6-FT TO 7-FT COVER SHALL BE INSULATED WITH 4-IN BLUEBOARD INSULATING MATERIAL.
4. SANITARY SEWER LESS THAN 6 FT TO 7-FT COVER SHALL INSULATE ON THREE SIDES (TOP AND BOTH SIDES) WITH 4-IN BLUEBOARD. SIDE INSULATION SHALL HAVE SPACERS RATS.
5. HORIZONTAL INSULATION UNDER ROADWAYS MUST BE 100-PSI RATED.

DATE <b>SEPT 2016</b>		SHEET <b>C11</b>	
<b>BLUE RIVER FLATS</b>			
<b>WATER &amp; SEWER DETAILS</b>			
<b>ICON</b> ENGINEERING INC. <small>10003, University Blvd, Suite 100, Fort Collins, CO 80525</small>		<b>SUSTAINABLE</b> SOLUTIONS <small>FOR THE WESTERN</small>	
UTILITIES RESEARCHED	DRAWN	CHECKED	DATE
PLAN DESIGN	CR	CR	DATE
APPROVED	DATE	REVISIONS	





Know what's below,  
Call before you dig.



HORIZ. SCALE: 1" = 20'  
VERT. SCALE: 1" = 5'

8720	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8715	EXISTING GRADE	INVERT	STA 8708.13	RES 24"	8700
8710	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8705	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8700	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8695	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8690	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700

HORIZ. SCALE: 1" = 20'  
VERT. SCALE: 1" = 5'

8715	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8710	EXISTING GRADE	INVERT	STA 8708.13	RES 24"	8700
8705	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8700	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8695	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8690	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700
8685	PROPOSED GRADE	INVERT	STA 8708.13	RES 24"	8700



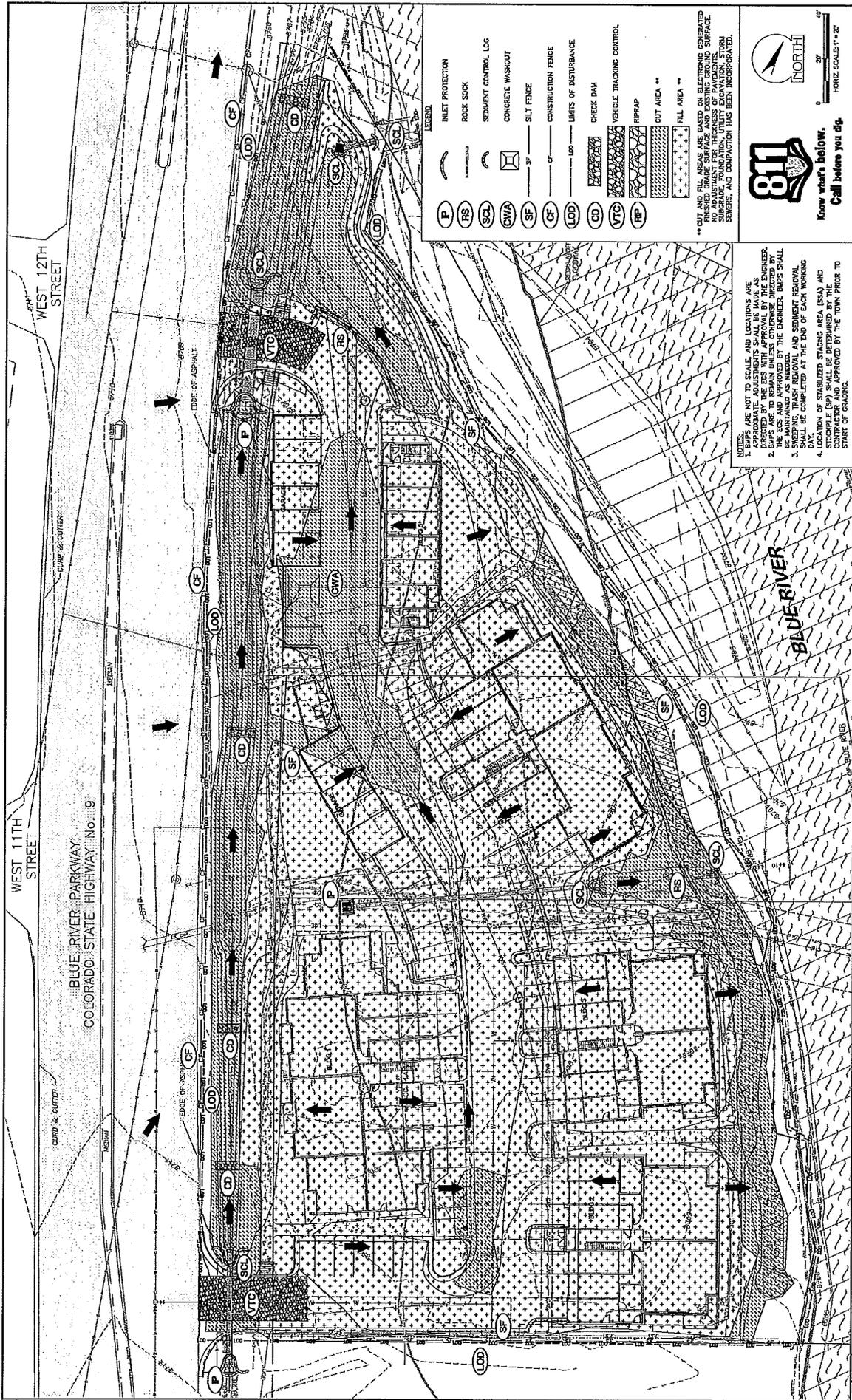
BLUE RIVER FLATS  
STORM SEWER PLAN & PROFILE 2

DATE  
SEPT 2018  
SHEET  
C13

PLANNING	DESIGNED	DRAWN	CHECKED
MAS	TWC	TWC	TWC

UTILITIES	RESEARCHED	APPROVED
RESEARCHED	CHECKED	APPROVED

NO.	DATE



- LEGEND**
- (P) INLET PROTECTION
  - (RS) ROCK SOCK
  - (SC) SEDIMENT CONTROL LOG
  - (CWA) CONCRETE WASHOUT
  - (SF) SILT FENCE
  - (CF) CONSTRUCTION FENCE
  - (LOD) LIMITS OF DISTURBANCE
  - (CD) CHECK DAM
  - (VTC) VEHICLE TRACKING CONTROL
  - (RP) REPRAP
  - CUT AREA \*\*
  - FILL AREA \*\*

**811**

Know what's below.  
Call before you dig.

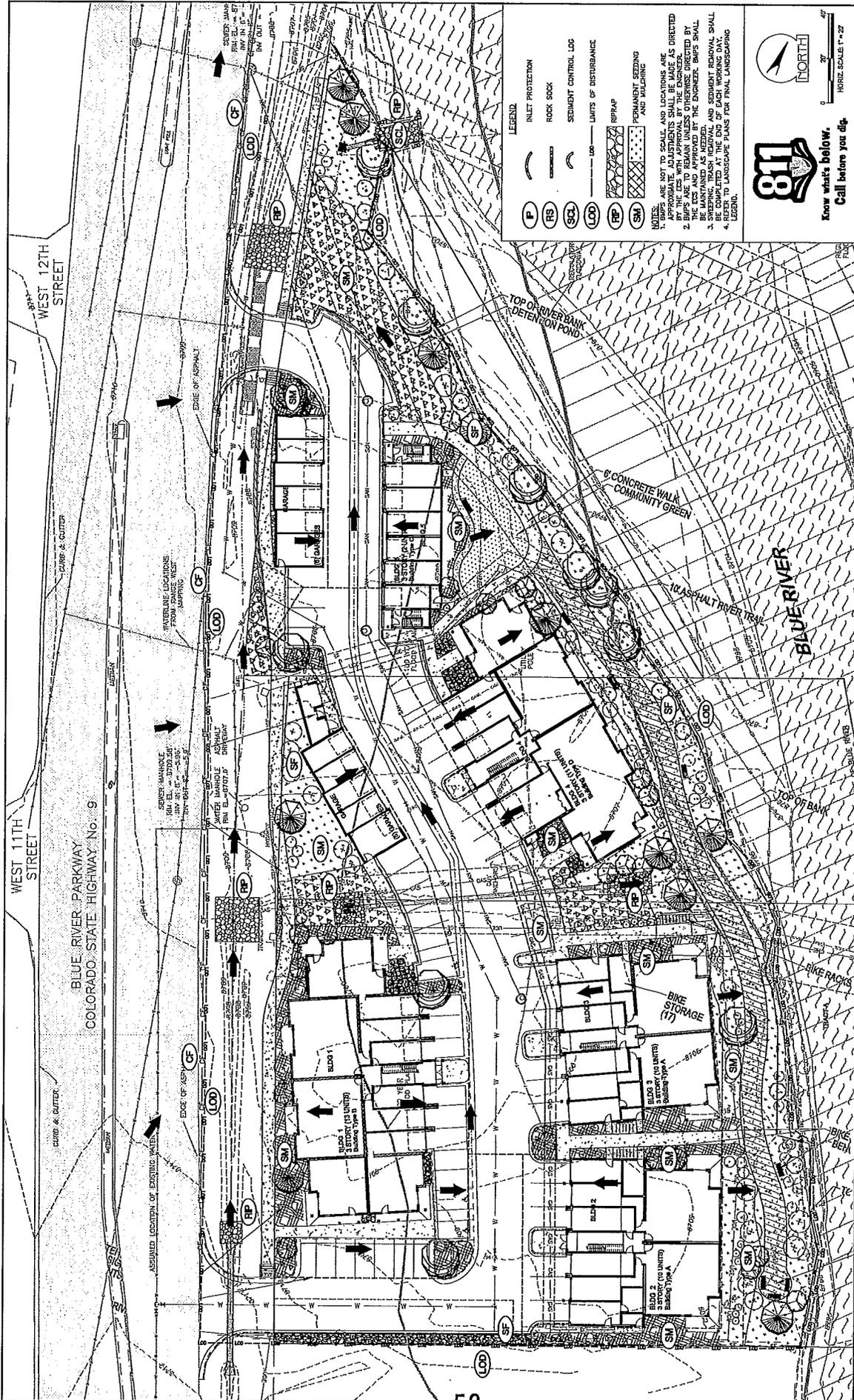
**811**

UTILITY LOCATIONS ARE NOT TO SCALE AND LOCATIONS ARE APPROXIMATE. ADJUSTMENTS SHALL BE MADE AS NECESSARY TO REFLECT FIELD CONDITIONS. THE E.S. AND APPROVED BY THE ENGINEER. BMPs SHALL BE COMPLETED AT THE END OF EACH WORKING DAY. LOCATION OF STABILIZED STAGING AREA (SSA) AND STOCKPILE (SP) SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE TOWN PRIOR TO START OF GRADING.

UTILITY LOCATIONS ARE NOT TO SCALE AND LOCATIONS ARE APPROXIMATE. ADJUSTMENTS SHALL BE MADE AS NECESSARY TO REFLECT FIELD CONDITIONS. THE E.S. AND APPROVED BY THE ENGINEER. BMPs SHALL BE COMPLETED AT THE END OF EACH WORKING DAY. LOCATION OF STABILIZED STAGING AREA (SSA) AND STOCKPILE (SP) SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE TOWN PRIOR TO START OF GRADING.

UTILITY LOCATIONS ARE NOT TO SCALE AND LOCATIONS ARE APPROXIMATE. ADJUSTMENTS SHALL BE MADE AS NECESSARY TO REFLECT FIELD CONDITIONS. THE E.S. AND APPROVED BY THE ENGINEER. BMPs SHALL BE COMPLETED AT THE END OF EACH WORKING DAY. LOCATION OF STABILIZED STAGING AREA (SSA) AND STOCKPILE (SP) SHALL BE DETERMINED BY THE CONTRACTOR AND APPROVED BY THE TOWN PRIOR TO START OF GRADING.

DATE SEPT 2018		SHEET C14	
<b>BLUE RIVER FLATS</b>			
EROSION AND SEDIMENT CONTROL - INITIAL & INTERIM			
<b>ICON ENGINEERING INC.</b>			
SILVERTHORNE COLORADO			
PLANNING		DESIGNED	
M.A.S.		C.O.C.	
RESEARCHED		DRAWN	
CHECKED		TWC	
APPR.		REVISIONS	



- LEGEND**
- (P) INLET PROTECTION
  - (RS) ROCK SOCK
  - (SCL) SEGMENT CONTROL LOG
  - (L) LIMITS OF DISTURBANCE
  - (RP) RPPRAP
  - (SM) PERMANENT SEEDING AND MULCHING
- NUMBERS ARE NOT TO SCALE AND LOCATIONS ARE APPROXIMATE. ADJUSTMENTS SHALL BE MADE AS DIRECTED BY THE FIELD SUPERVISOR. APPROVALS SHALL BE OBTAINED BY THE ENGINEER AND APPROVED BY THE ENGINEER. BMPs SHALL BE MAINTAINED AS NEEDED. AND SEDIMENT REMOVAL SHALL BE COMPLETED AT THE END OF EACH WORKING DAY. REFER TO LANDSCAPE PLANS FOR FINAL LANDSCAPING LEGEND.

**811**

Know what's below.  
Call before you dig.

**ICON ENGINEERING, INC.**  
1400 SOUTH GARDEN AVENUE, SUITE 100  
DENVER, CO 80202

DATE: SEPT 2016  
SHEET: C15

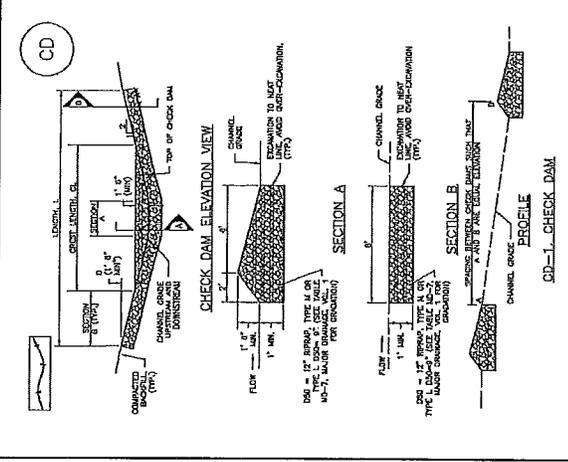
PROJECT: BLUE RIVER FLATS  
EROSION AND SEDIMENT CONTROL - FINAL

SCALE: 1" = 20'

NORTH

PLAN	RESEARCHED	UTILITIES	RESEARCHED
DRAWN	MAS	DRAWN	DRAWN
DESIGNED	CS	CHECKED	CHECKED
CHECKED	TWC		
APPR.			
DATE			
NO.			

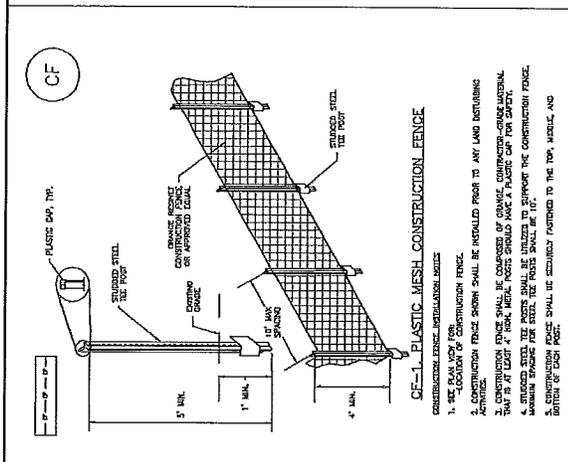




**CD-1. CHECK DAM**

**GENERAL NOTES:**

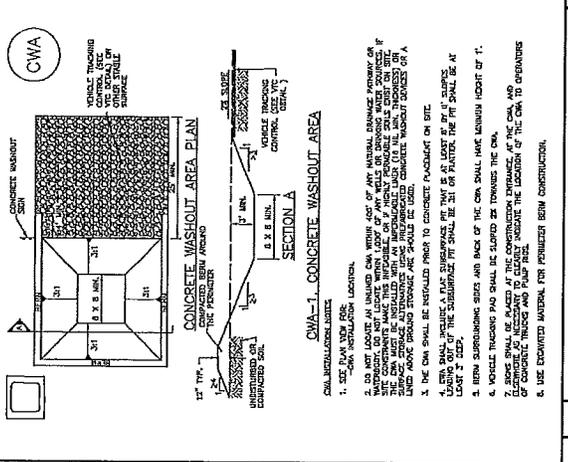
1. SEE PLAN VIEW FOR DIMENSIONS.
2. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LOCAL JURISDICTION.
3. THE DAM SHALL BE CONSTRUCTED WITH A MINIMUM OF 1' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
4. CHECK DAMS SHALL BE INSTALLED AFTER CONSTRUCTION OF THE CHANNEL AND BEFORE THE CHANNEL IS OPENED TO FLOW.
5. CHECK DAMS SHALL BE CONSTRUCTED WITH A MINIMUM OF 1' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
6. CHECK DAMS SHALL BE CONSTRUCTED WITH A MINIMUM OF 1' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
7. CHECK DAMS SHALL BE CONSTRUCTED WITH A MINIMUM OF 1' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
8. CHECK DAMS SHALL BE CONSTRUCTED WITH A MINIMUM OF 1' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
9. CHECK DAMS SHALL BE CONSTRUCTED WITH A MINIMUM OF 1' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
10. CHECK DAMS SHALL BE CONSTRUCTED WITH A MINIMUM OF 1' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.



**CF-1. PLASTIC MESH CONSTRUCTION FENCE**

**GENERAL NOTES:**

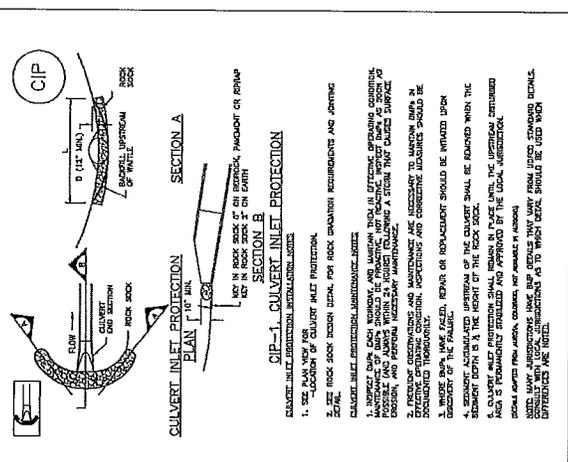
1. SEE PLAN VIEW FOR DIMENSIONS.
2. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LOCAL JURISDICTION.
3. THE FENCE SHALL BE CONSTRUCTED WITH A MINIMUM OF 5' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
4. THE FENCE SHALL BE CONSTRUCTED WITH A MINIMUM OF 5' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
5. THE FENCE SHALL BE CONSTRUCTED WITH A MINIMUM OF 5' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
6. THE FENCE SHALL BE CONSTRUCTED WITH A MINIMUM OF 5' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
7. THE FENCE SHALL BE CONSTRUCTED WITH A MINIMUM OF 5' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
8. THE FENCE SHALL BE CONSTRUCTED WITH A MINIMUM OF 5' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
9. THE FENCE SHALL BE CONSTRUCTED WITH A MINIMUM OF 5' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
10. THE FENCE SHALL BE CONSTRUCTED WITH A MINIMUM OF 5' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.



**CWA-1. CONCRETE WASHOUT AREA**

**GENERAL NOTES:**

1. SEE PLAN VIEW FOR DIMENSIONS.
2. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LOCAL JURISDICTION.
3. THE WASHOUT AREA SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
4. THE WASHOUT AREA SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
5. THE WASHOUT AREA SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
6. THE WASHOUT AREA SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
7. THE WASHOUT AREA SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
8. THE WASHOUT AREA SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
9. THE WASHOUT AREA SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
10. THE WASHOUT AREA SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.



**CIP-1. CULVERT INLET PROTECTION**

**GENERAL NOTES:**

1. SEE PLAN VIEW FOR DIMENSIONS.
2. CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE LOCAL JURISDICTION.
3. THE PROTECTION SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
4. THE PROTECTION SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
5. THE PROTECTION SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
6. THE PROTECTION SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
7. THE PROTECTION SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
8. THE PROTECTION SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
9. THE PROTECTION SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.
10. THE PROTECTION SHALL BE CONSTRUCTED WITH A MINIMUM OF 12' OF HEIGHT FROM THE CENTER OF THE CHECK DAM.

NO.	DATE	REVISION	APPR.

PLAN	UTILITIES
DESIGNED	RESEARCHED
CHECKED	DRAWN
TWC	CHECKED

**ICON ENGINEERING INC.**  
 1000 S. W. 10TH AVE., SUITE 100  
 MIAMI, FL 33135  
 (305) 551-1111  
 WWW.ICONENGINEERING.COM









**NOT FOR  
 CONSTRUCTION**

DATE:  
 DRAWING PREPARED BY: LATEL PAW  
 CHECKED BY: LATEL PAW  
 DESIGNED BY: LATEL PAW  
 02/22/16 FINAL SITE PLAN

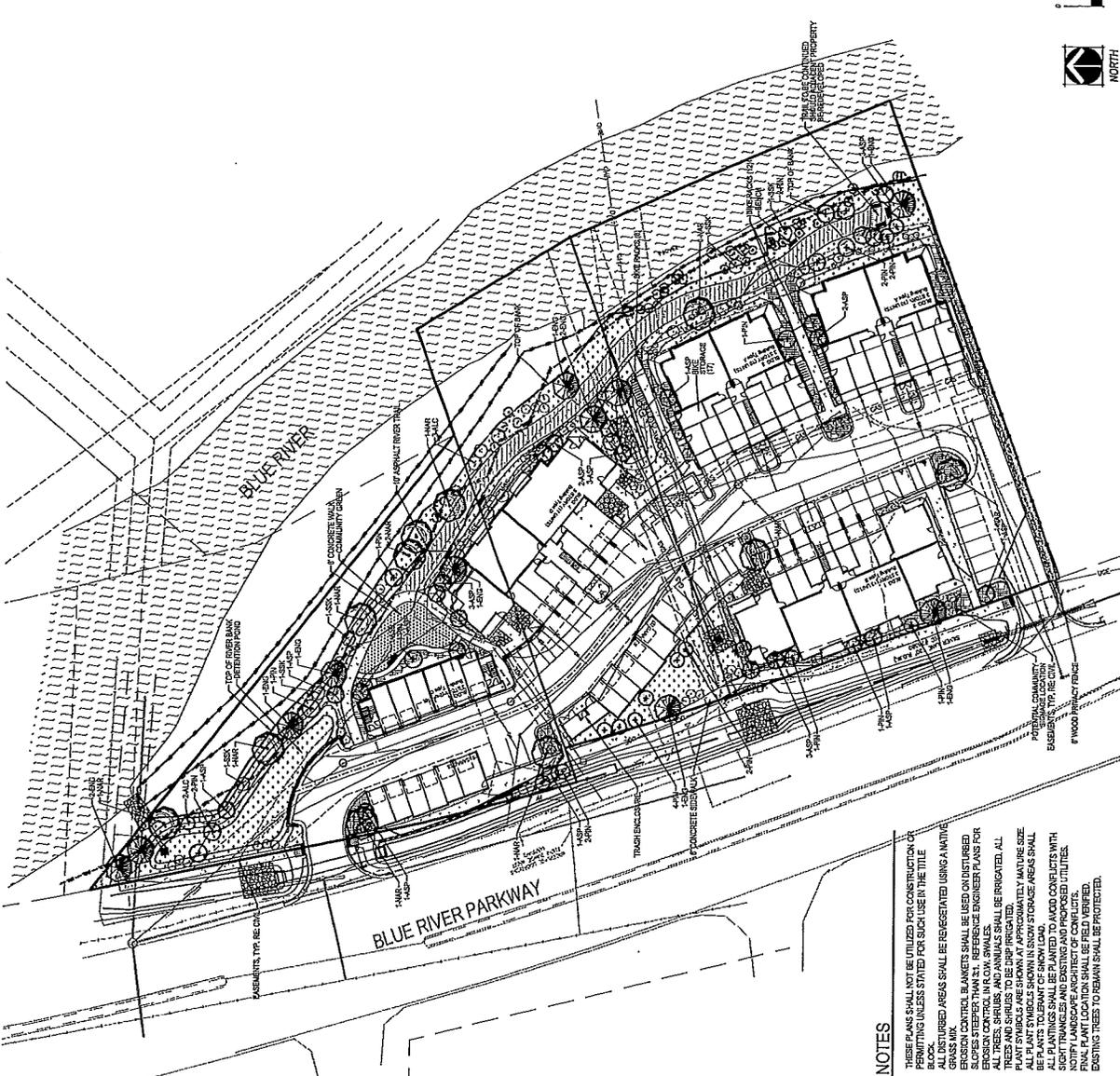
SHEET TITLE:  
 LANDSCAPE  
 PLAN  
 SHEET NUMBER:  
 L-2

- LEGEND**
- EVERGREEN TREES
  - DECIDUOUS TREES
  - EXISTING TREES TO REMAIN
  - SHRUBS
  - ORNAMENTAL GRASS
  - NATIVE GRASS SEED
  - CRUSHED FINES
  - CONCRETE
  - RIVER ROCK COBBLE MULCH
  - SOD LAWN
  - LANDSCAPE BED
  - ASPHALT PAVING

**LANDSCAPE CALCULATIONS**

SITE AREA	91,821 SF
TOTAL REQ'D LANDSCAPE	27,276 SF
5% XERISCAPE REDUCTION	26,197 SF
TREES REQ'D:	10,476 SF / 187 TREES
TOTAL TREES PROPOSED:	80
TOTAL SHRUBS PROPOSED:	7
DECIDUOUS	49
CONIFEROUS	31
SHRUBS REQ'D:	2,820 SF / 131 SHRUBS
TOTAL SHRUBS PROPOSED:	154

NOTE:  
 1. ALL EXISTING TREES TO BE PRESERVED WHERE POSSIBLE.  
 EXISTING TREES ARE NOT BEING CREDITED TOWARDS REQUIRED TREES.  
 2. EACH BECH IS EQUAL TO A SUBSTITUTION OF 1 TREE PER TOWN CODE.



**NOTES**

1. THESE PLANS SHALL NOT BE UTILIZED FOR CONSTRUCTION OR PERMITTING UNLESS STATED FOR SUCH USE IN THE TITLE.
2. ALL DISTURBED AREAS SHALL BE REVEGETATED USING A NATIVE GRASS MIX.
3. ALL DISTURBED AREAS SHALL BE REVEGETATED USING A NATIVE GRASS MIX.
4. ALL TREES, SHRUBS, AND ANNUALS SHALL BE IRRIGATED. ALL PLANT SYMBOLS SHOWN IN SNOW STORAGE AREAS SHALL BE PLANTS TOLERANT OF SNOW LOAD.
5. ALL PLANT SYMBOLS SHOWN IN SNOW STORAGE AREAS SHALL BE PLANTS TOLERANT OF SNOW LOAD.
6. ALL PLANT SYMBOLS SHOWN IN SNOW STORAGE AREAS SHALL BE PLANTS TOLERANT OF SNOW LOAD.
7. SIGHT TRIANGLES AND EXISTING AND PROPOSED UTILITIES.
8. NOTIFY LANDSCAPE ARCHITECT OF CONFLICTS.
9. FINAL PLANT LOCATION SHALL BE FIELD VERIFIED.
10. EXISTING TREES TO REMAIN SHALL BE PROTECTED.

**LEGEND**

- EVERGREEN TREES
- DECIDUOUS TREES
- EXISTING TREES
- SHRUBS
- ORNAMENTAL GRASS
- WAXY GRASS (SEED)
- CRUISER PINES
- CONCRETE
- RIVER ROCK COBBLE MULCH
- SNOW STORAGE AREAS
- SOIL LAWN
- LANDSCAPE BED
- ASPHALT PAVING

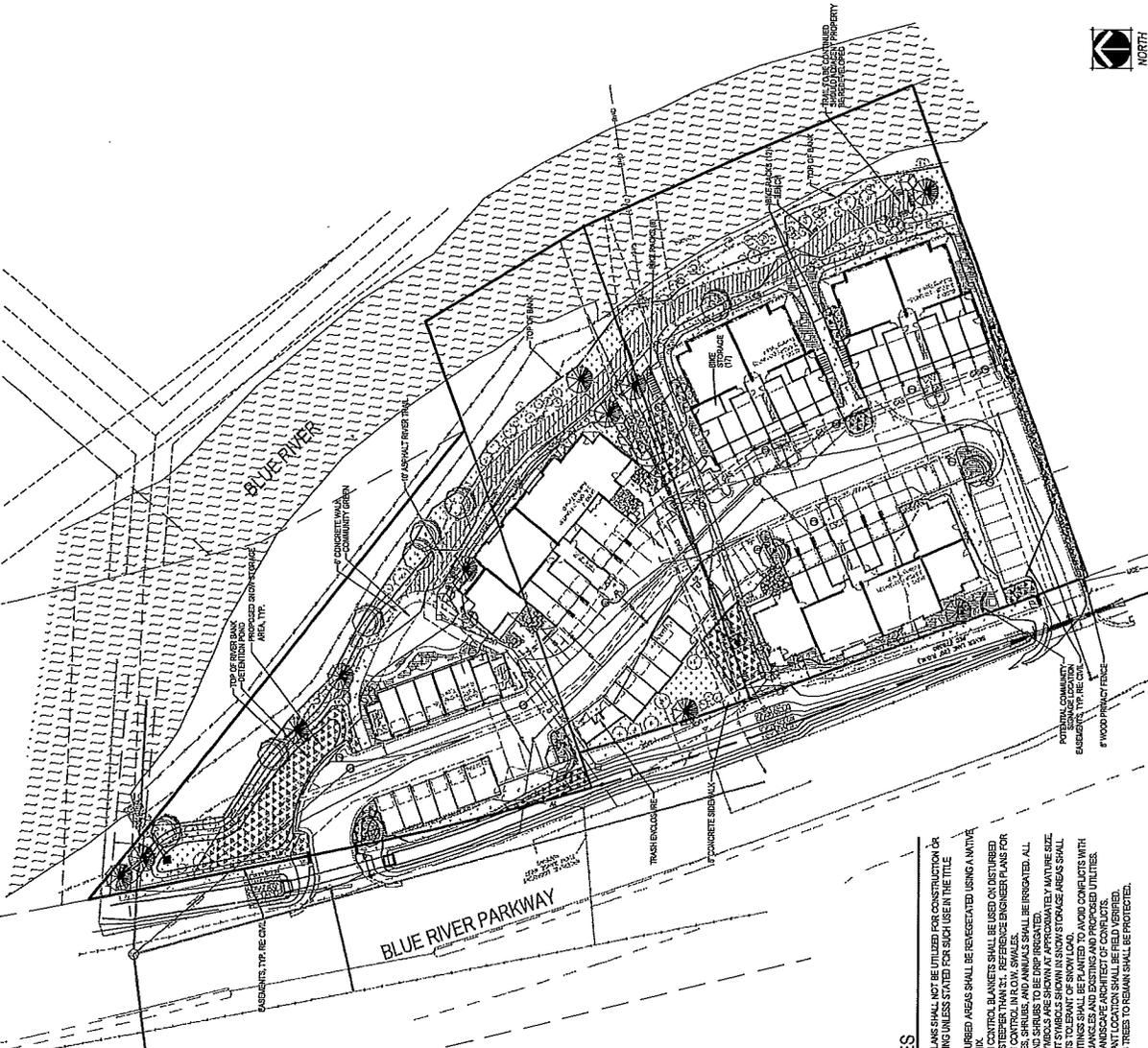
**SNOW STORAGE CALCULATIONS**

TOTAL PARKING AREA: 2,688 SQ. FT.  
 SNOW STORAGE PROVIDED: 6,337 SQ. FT.

**PARKING CALCULATIONS**

CATEGORY	PARKING REQ'D PER UNIT	UNIT QTY	PARKING REQ'D
1 BEDROOM	1.5	10	15
2 BEDROOM	2	35	70
3 BEDROOM	2.5	1	2.5
VISITOR	1 PER 5 UNITS	48	8.2
<b>SUBTOTAL</b>			<b>95.7</b>
5% BIKE PARKING REDUCTION (55 SPACES / 1,100)			-4.8
<b>SUBTOTAL</b>			<b>90.9</b>
HANDICAP			4
<b>TOTAL</b>			<b>95.9</b>

PARKING PROVIDED	TYPE	PROVIDED
SURFACE		55
DETACHED COVERED		12
GARAGE		29
<b>TOTAL</b>		<b>96</b>



- NOTES**
- THESE PLANS SHALL NOT BE UTILIZED FOR CONSTRUCTION OR PERMITTING UNLESS STATED FOR SUCH USE IN THE TITLE.
  - ALL DISTURBED AREAS SHALL BE REVEGETATED USING A NATIVE GRASS MIX.
  - EROSION CONTROL BLANKETS SHALL BE USED ON DISTURBED AREAS UNTIL PERMANENT VEGETATION IS ESTABLISHED.
  - ALL TREES, SHRUBS, AND ANNUALS SHALL BE IRRIGATED. ALL TREES AND SHRUBS TO BE DRIP IRRIGATED.
  - ALL PLANT SYMBOLS SHOWN IN SNOW STORAGE AREAS SHALL BE PLANTS TOLERANT OF SNOW LOAD.
  - ALL PLANTINGS SHALL BE PLANTED TO AVOID CONFLICTS WITH EXISTING UTILITIES.
  - NOTIFY LANDSCAPE ARCHITECT OF CONFLICTS.
  - FINAL PLANT LOCATION SHALL BE FIELD VERIFIED.
  - EXISTING TREES TO REMAIN SHALL BE PROTECTED.





Godden|Sudik  
ARCHITECTS

Final Site Application: Blue River 50 – Silverthorne  
Referral Agency RESPONSES:

**Planning Department Review Comments & Response:**

**Planning Department Review Comments** - Staff has reviewed the application for a Minor Subdivision and Final Site Plan for Blue River Flats. Please submit the following revisions:

**Minor Subdivision** – The purpose of the minor subdivision is to combine the three lots in the project site, reconfigure the easements, and create a Tract that will be dedicated to the Town.

- First page – The first page of the plat appears to show existing conditions. This is not necessary. Please show the proposed plat on one page with all the certifications, or show only the certifications on the first page. *RESPONSE: Comment Noted, Certificates are shown on page one, and the proposed plat is shown on page two.*
- Certificates – Please add a section to the owner’s certificate to clearly state that, by virtue of this plat, Tract A is hereby dedicated to the Town of Silverthorne. *RESPONSE: Section added.*
- Scale – The scale on Page 2 is not correct. Please revise. *RESPONSE: Scale has been corrected.*
- General Note on Easements – the different hatching patterns for the easements create a lot of clutter and reduce legibility of the plat. Please consider less busy hatching patterns and shading such that the plat and easement labels are easy to read. *RESPONSE: Hatch patterns adjusted for clarity.*
- Please create a ‘Lot 1’ (as an example) for the combined parcels, with the exact acreage. Please do the same for Tract A. Both Lot 1 and Tract A should be bounded with the same line heaviness, so that it’s clear that each is separate and stands alone from the other. *RESPONSE: Lot 1 and Track A has been created with the same heavy boundary lines.*
- Pedestrian Easement – Please label this easement ‘Multi-Use Recreational Trail Easement’. It should be clearly noted that the easement is 25 feet wide, and to scale. *RESPONSE: Trail has been re-labeled and a dimension has been added.*
- Access Easement – the access easement should be from the south property line, for a width of 24 feet. Please revise. *RESPONSE: Easement has been revised.*

Drainage Easement – the proposed drainage easement in the northernmost corner of the property is being shown in conjunction with the recreational trail easement. Please explain the need for this drainage easement, and how this portion of the property will function in the event that the Town would want to extend to trail to the sidewalk on HWY 9. The Town may accept a detention pond as proposed, but has concerns with combining that portion of the recreational trail easement with a drainage



Godden|Sudik  
ARCHITECTS

easement. Please clarify. *RESPONSE: The recreational trail easement has been taken out from the drainage easement. The drainage easement is for the water quality pond that is in place to enhance storm runoff from this improvements.*

- A portion of the gas line is not shown within the center utility easement. Please ensure that the utility easement that is shown in conjunction with the drainage easement captures the entirety of the gas line. See Utility Plan. *RESPONSE: The drawing has been revised to show a utility easement that captures the entirety of the proposed gas line.*
- A 5-foot utility easement is shown on the west portion of the unsubdivided 'triangle' parcel on the north end of the site. Is a 5-foot utility easement being created in that location? If so, what is the purpose of the easement? *RESPONSE: This easement was left over from the preliminary plat, and is not required on the Final Plat, it has been removed.*
- Please remove all labels that reference a 5-foot setback. *RESPONSE: Labels removed.*
- Water & Sewer Easement – the northern portion of this easement should be labeled 'Sanitary Sewer', not water. This is also true for the northern portion of the water line easement that is incorrectly labeled 'Sanitary Sewer'. *RESPONSE: Labels updated.*
- Plat note - Please add a plat note that reads: "No below grade improvements are allowed within the exclusive water and sewer easement without the written consent of the Town of Silverthorne". *RESPONSE: Note added.*
- Certificates – Please reorganize such that the Clerk and Recorder's certificate is on the bottom right hand corner. *RESPONSE: Clerk and Recorder's certificate has been moved to the bottom right corner.*
- Certificates – Please retitle treasurer's certificate to "Certificate of Taxes Paid". *RESPONSE: Treasurer's Certificate has been retitled.*
- Clerk and Recorder Certificate – Please revise to read as follows: *RESPONSE: Clerk and Recorder Certificate has been revised.*

CLERK AND RECORDER'S CERTIFICATE

This plat was accepted for filing in the office of the Summit County Clerk and Recorder on this \_\_\_\_ day of \_\_\_\_\_, 20\_\_ A.D., at \_\_\_\_\_ a.m./p.m., and was recorded at Reception Number \_\_\_\_\_.

\_\_\_\_\_  
(Signature)  
County Clerk and Recorder



Godden|Sudik  
ARCHITECTS

- Surveyor Certificate – Please revise the surveyor certificate such that it reads as follows:  
*RESPONSE: Surveyor's Certificate has been revised.*

SURVEYOR'S CERTIFICATE

I do hereby certify that I am a registered Land Surveyor licensed under the laws of the State of Colorado, and do hereby certify that this plat is true, correct and complete as laid out, platted, dedicated and shown hereon, that such plat was made from an accurate survey of said property by me and under my supervision and correctly shows the location and dimensions of the lots, easements and streets of said subdivision as the same are staked upon the ground in compliance with applicable regulations governing the subdivision of land.

In witness thereof I have set my hand and seal this \_\_\_\_ day of \_\_\_\_\_ A.D., 20 \_\_\_\_.

\_\_\_\_\_  
(Signature)

Colorado Registered  
Professional Land Surveyor

(Surveyor's Name)

Colorado Registration  
# \_\_\_\_\_

**Final Site Plan** – Below are the comments related to the Final Site Plan:

- Parking – The site plan no longer shows the designated handicap parking spaces and associated striping. Please show the handicap spaces and striping clearly on the site plan. *RESPONSE: Handicap spaces have been clearly labeled on the site plan.*
- Site Plan – Please show the edges of the roof eaves on the site plan. This information is necessary to ensure that all building elements are within the property and meet the setbacks. *RESPONSE: Site Plan has been updated to show roof eaves.*
- CDOT Drainage Easement – It appears that the 30-foot drainage easement is proposed to remain in place. The proposed piping is within that 30-foot easement. However, the applicant is proposing a garage building within the drainage easement. Buildings cannot be located on easements. Please revise or clarify, or modify the drainage easement. *RESPONSE: Site plan has been revised to remove the garage building from the drainage easement.*



Godden|Sudik  
ARCHITECTS

- Landscaping – Staff counted approximately 20 trees located within the utility easement directly behind Buildings 2, 3, and 4. Please revise the Landscape Plan to address:
  - Standard 3.5.3 of the Riverfront District Design Standards and Guidelines states, “*Significant landscape materials, such as trees, shall be located outside utility easements. Planting trees over utility lines is prohibited*”.

RESPONSE: *Trees within the easement have been relocated.*

- Landscape Plan – Per Section 4-6-11.g.6, existing healthy trees can be credited for required landscaping. The applicant can consider this credit if needed.

RESPONSE: *Comment noted.*

Landscape Plan – Per Section 4-6-11.g.23, up to 10% of the required number of trees may be substituted for alternative forms of landscaping, including benches, picnic tables, etc. The applicant is proposing 6 benches, which may be substituted for required trees, if requested.

RESPONSE: *Thank you. This is not currently being requested.*

- Overhead Utilities – The applicant has requested that the Town consider a RETA that can be used at a future date to construct a bridge and fully underground the existing overhead utilities across the River. In addition, the applicant is proposing to dedicate Tract A, which encompasses the portion of the Blue River that is on the property to the Town.

Staff can support this arrangement, but only if the RETA applies to all sales of the condominiums, including the first sale. Please see the attached template of the RETA, prepared by the Town Attorney. RESPONSE: *Owners are working with the Town to come to a final agreement for the RETA, and have prepared models to discuss.*

- Elevations – Staff noted that the highest ridge elevations of the five buildings are now approximately 2 feet higher. Although it appears that the height maximums are being met, please explain what caused the changes in the heights of the buildings. RESPONSE: *The original calculations for building heights were measured from the finish floor of the building, not from existing grade. This was a technical mistake, and has been adjusted to show the overall height as measured from average existing grade.*
- Architecture – Building C – At the board meetings for the Preliminary Site Plan, the architecture of Building C was discussed. The applicant indicated that they would make changes to address the comments. What changes were made to the design of Building C? RESPONSE: *Building C has been enhanced more windows, added more stone to the front elevation, and scaled down the massing to more of a human scale.*



Godden|Sudik  
ARCHITECTS

**Fire Department Review Comments & Response:**

1. The proposed fire hydrants (3) shown on Sheet C7 is acceptable to the fire department. Fire hydrants shall be installed, tested, and accepted by the water department prior to any combustible construction on the site. Bollard protection may be required for all fire hydrants. *RESPONSE: Comment noted.*
2. Except for the South entrance, shown as 24 feet, the civil drawing shall indicate project road width at 26 feet. This will accommodate aerial apparatus access and minimize confusion during site construction. Overhead utility and powerlines shall not be located over aerial apparatus access road or between the aerial apparatus access road and the buildings. *RESPONSE: Dimensions have been added to more clearly show the road widths.*
3. An approved fire sprinkler system is required in all buildings for this project except the stand-alone garages. Size waterlines into each building to meet fire sprinkler demand and domestic water needs. Have fire sprinkler contractor contact fire department for permit details. *RESPONSE: Comment noted.*
4. An approved fire alarm system is required for each building. Have fire alarm contractor contact fire department for permit details. *RESPONSE: Comment noted.*
5. An approved standpipe system shall be installed in all buildings where the floor level of the highest story is more than 20 feet above the lowest level of fire department vehicle access. *RESPONSE: Comment noted.*
6. Site landscaping shall not visually or physically obstruct access to fire hydrants and other fire appurtenances. *RESPONSE: Proposed landscape meets this requirement.*
7. Remove snow storage area around fire hydrant at Southeast corner of Building 1. *RESPONSE: The snow storage plan has been updated.*
8. Suggest a meeting with the developer to address the issues outlined above. *RESPONSE: Comment noted, applicant team will set up a meeting prior to building permit submittal.*

**Utility Department Review Comments & Response:**

**General:** The Town of Silverthorne has sufficient water and sewer capacities in the adjacent right of way to serve the proposed project.

Notes on Plans:

---

Godden|Sudik Architects

Plaza Quebec  
6025 S. Quebec St., Suite 375  
Centennial, Colorado 80111

ph 303.455.4437  
fx 303.477.5680  
goddeosudik.com



Godden|Sudik  
ARCHITECTS

1. The Civil Engineer has been great to work with! Design is much improved over what we first saw – we will make minor changes prior to construction (5a and 7a below)
2. Sheet C1
  - a. Garage building with trash enclosure extends into PSCo easement *RESPONSE: Garage building has been removed from the easement.*
  - b. Trail on West side is only 6 feet wide, and is against (and under the overhang of) the north garage building *RESPONSE: Snow guards will be used on the roof to prevent snow from falling onto the path.*
  - c. The north garage building encroaches into a depicted setback *RESPONSE: The garage building is no longer shown in the setback, a 0' setback is being utilized at this location per town code requirements.*
  - d. The river trail should connect to the highway trail instead of stopping at the east side of the north parking lot in the future. Drainage Easement in that location? *RESPONSE: The river trail connects to the community green and provides a crossing to the sidewalk trail mid-site.*
3. Sheet C2
  - a. There are two, not just one, active 8" ductile iron pipe crossings of Hwy 9 at the north end of the project, to improve reliability, this project will connect to both *RESPONSE: Comment noted.*
4. Sheet C7
  - a. The hydrant at the north entrance should be moved to the east of the trail or connected more directly - straight west - to the water main and insulated *RESPONSE: Hydrant is now connected to the existing water directly west.*
  - b. Move hydrants back to at least 3' behind curbs *RESPONSE: Hydrants moved to be at least 3' behind curbs.*
  - c. Bollards are not required on the building side of hydrants *RESPONSE: Comment noted.*
5. Sheet C10
  - a. A high point is created in the water main which would require an air relief valve, instead maintain depth from Station 6+00 to Station 7+30 to eliminate two vertical offsets and the air relief at the high point *RESPONSE: Comment noted.*
6. Sheet C11
  - a. Water Note 8: "Fire Hydrant" [we agree?] There may have been more to this note that was omitted? *RESPONSE: Note revised.*
7. Sheet L-2
  - a. Remove trees from within 10 feet of all water mains, hydrants, and sewer mains *RESPONSE: The landscape plan has been updated.*
8. Sheet L-3
  - a. No snow storage permitted within 5 feet of fire hydrants



Godden|Sudik  
ARCHITECTS

*RESPONSE: The snow storage plan has been updated.*

**SPORT Committee Review Comments & Response:**

SPORT is concerned about the lack of pedestrian crossing opportunities for over a mile to the north and ¼ mile to the south. This creates a dangerous situation where pedestrians will cross at random points. More acceptable pedestrian crossing areas should be proposed.

*RESPONSE: The team agrees that a crossing area would reduce potentially dangerous crossing situations across the highway, however this request is out of the scope of this project.*

**Public Works Department Review Comment & Response:**

1) Development within the floodplain and FEMA permitting.

Most of the project site is located within the FEMA mapped 100-year floodplain.

Development within the floodplain is permissible as long FEMA, State and local standards can be met. The two main criteria include 1) elevating the lowest floor elevations to be at least one (1) foot above the FEMA mapped and published Base Flood Elevations (BFEs), and 2) performing scientific analysis and modeling to evaluate the effects that proposed fill would have on water surface elevations. Per State and local standards, development is allowed as long as it can be demonstrated, via scientific modeling, that changes to BFEs are equal to or less than 0.5 feet.

The Applicant provided a floodplain analysis performed by Wright Water Engineers (WWE), dated February 15, 2016. The study concluded, that the post-project maximum change to the floodway water surface elevations would be 0.29 feet higher, which is less than the 0.5 foot threshold – and thus allowed by FEMA, State and local regulations.



Godden|Sudik  
ARCHITECTS

FEMA has since reviewed and approved the study and has issued the CLOMR-F permit, which stands for “Conditional Letter of Map Revision based on Fill.” This permit is FEMA’s official approval of the proposal to fill the site for development per the current site plan. Silverthorne Staff required the CLOMR-F as a condition of approval at Preliminary – and this has been satisfied.

With respect to building elevations, all are shown to meet or exceed the one (1) foot minimum elevation above the BFE. This will also be confirmed by survey and Elevation Certificates during construction.

*RESPONSE: Comment noted.*

2) Proposed encroachments into Xcel easements.

The site is encumbered with several different easements. Of particular interest are two (2) Xcel utility easements which proposed buildings are shown to encroach into. At Preliminary review, Silverthorne staff required that the Applicant obtain permission from the easement Owner, Xcel for these encroachments in order for to move forward to Final. The Applicant has since provided an email from Xcel stating that Xcel has no objections to the proposed encroachments.

*RESPONSE: Comment noted.*

3) Access / CDOT Access Permit

The Applicant has received from CDOT and has provided to the Town two (2) Access Permits for the project – one for each the northern and southern access points. CDOT has accepted the current access points and configurations and is not requiring any additional highway improvements such as accel/decel lanes, turning movement restrictions or anything else.

*RESPONSE: Comment noted.*

4) Grading and Drainage

Staff has reviewed the Grading and Drainage Plan, and finds that it meets the Town’s requirements.

*RESPONSE: Comment noted.*

5) Plat – Tract A dedication

Please specify on the plat that Tract A will be dedicated to the Town.



*RESPONSE: Comment noted, drawings revised.*

6) Sidewalk and Landscape Wall proposed within CDOT right-of-way

CDOT permission and the appropriate permitting for these improvements will be required.

The sidewalk will be privately maintained until such time that it connects to a larger, contiguous Town path system.

It is recommended that the sidewalk be at least a few feet away from the buildings for snow maintenance / plowing activities.

*RESPONSE: CDOT has expressed that they have no issues with the small amount of sidewalk that is in the Right-Of-Way. The applicant team is actively pursuing a permit for this sidewalk with CDOT.*

**US Army Corps of Engineers Review Comments & Response:**

Regulatory Division SPK-2016-00174

Attn: Lina M. Lesmes  
Town of Silverthorne  
601 Center Circle  
Silverthorne, Colorado 80498

Dear Ms. Lesmes

---

Godden|Sudik Architects

Plaza Quebec  
6025 S. Quebec St., Suite 375  
Centennial, Colorado 80111

ph 303.455.4437  
fx 303.477.5680  
goddensudik.com



Godden|Sudik  
ARCHITECTS

We are responding to your request for comments on the Blue River Flats project. The project site is located along the Blue River, Latitude 39.643814°, Longitude -106.077666°, Town of Silverthorne, Summit County, Colorado.

The Corps of Engineers' jurisdiction within the study area is under the authority of Section 404 of the Clean Water Act for the discharge of dredged or fill material into waters of the United States. Waters of the United States include, but are not limited to, rivers, perennial or intermittent streams, lakes, ponds, wetlands, marshes, wet meadows, and seeps. Project features that result in the discharge of dredged or fill material into waters of the United States will require Department of the Army authorization prior to starting work.

To ascertain the extent of waters on the project site, the applicant should prepare a wetland delineation, in accordance with the "Minimum Standards for Acceptance of Preliminary Wetlands Delineations" and "Final Map and Drawing Standards for the South Pacific Division Regulatory Program" under "Jurisdiction" on our website at the address below, and submit it to this office for verification. A list of consultants that prepare wetland delineations and permit application documents is also available on our website at the same location.

The range of alternatives considered for this project should include alternatives that avoid impacts to wetlands or other waters of the United States. Every effort should be made to avoid project features which require the discharge of dredged or fill material into waters of the United States. In the event it can be clearly demonstrated there are no practicable alternatives to filling waters of the United States, mitigation plans should be developed to compensate for the unavoidable losses resulting from project implementation.

Please refer to identification number SPK-2016-00174 in any correspondence concerning this project. If you have any questions, please contact me at our Grand Junction Regulatory Office, 400 Rood Avenue, Room 224, Grand Junction, Colorado 81501, by email at [Matthew.R.Montgomery@usace.army.mil](mailto:Matthew.R.Montgomery@usace.army.mil), or telephone at 970-243-1199 Ext. 1017. For more information regarding our program, please visit our website at [www.spk.usace.army.mil/Missions/Regulatory.aspx](http://www.spk.usace.army.mil/Missions/Regulatory.aspx).

Matt Montgomery  
US Army Corps of Engineers  
Senior Project Manager, CO West Branch  
Sacramento District  
400 Rood Avenue, Room 224  
Grand Junction, Colorado 81501  
970-243-1199 Ext: 1017

---

Godden|Sudik Architects

Plaza Quebec  
6025 S. Quebec St., Suite 375  
Centennial, Colorado 80111

ph 303.455.4437  
fx 303.477.5680  
[goddensudik.com](http://goddensudik.com)



*RESPONSE: Comments noted, Our development within the depicted Study Area avoids impacts (i.e. the discharge of dredged or fill material) to the mapped wetlands and other waters, so it is not necessary to obtain Section 404 of the Clean Water Act authorization from the U.S. Army Corps of Engineers.*

### **Park & Wildlife Review Comments & Response:**

#### Summary of Comments:

"CPW encourages Blue River Flats and Town of Silverthorne Planning Department to afford the highest protection for Colorado's wildlife species and habitats.

CPW reviewed the Blue River Flats application materials, and found that reports did not include a wildlife report or wetlands report. A wetlands study area map was included but was not accompanied by any description of the study or plans for protection. CPW is specifically concerned about the proposed site plan that appears to eliminate all of the existing riparian vegetation along the Blue River, and build structures and a paved path within the existing wetland/riparian buffer. This proposal is out of character with existing residential development along the Blue River in Silverthorne the adjacent properties all maintain a buffer of trees and shrubs along the bank of the river.

The final site plans do not include a wetlands buffer of any distance, and in fact shows the entirety of the wetlands and riparian habitat being cut and developed into a "community green" area including a paved walking path.

CPW recommends a minimum of 50' wetland buffer for all development, including building sites, roads, parking lots, and paths to reduce impacts and maintain some ability for wildlife to pass through the area. CPW also recommends eliminating the proposed community green area and paved path, and instead maintaining the existing riparian shrubs and trees. CPW understands that this amendment will require a reduction in density of units and parking spaces to reduce the overall footprint of the project, as the proposed site plan utilized a majority of the space on the property including the riparian zone, If the existing wetland/riparian habitat is disturbed or impacted, CPW requests that mitigation occur to create new wetland habitat equal of greater than the permanently impacted area. CPW further recommends that unless necessary for fire mitigation, no landscaping live tree removal or native vegetation removal be permitted within the buffer zones.

Potential impacts to bird species due to tree and vegetation removal along the blue river are of concern to CPW.

CPW recommends that all new housing developments in Summit County, including Blue River Flats, require and enforce the following overnight garbage restrictions...



Godden|Sudik  
ARCHITECTS

...CPW advises the Town of Silverthorne and the Blue River Flats HOA to incorporate the above trash policies into their requirements, covenants and restrictions to help prevent future bear conflicts in the proposed development.

CPW is concerned about an increase in conflicts with moose.

CPW recommends that the Blue River Flats HOA require that pets must be controlled with leashes while on the property, prohibit dogs and cats from roaming freely, and require pets to be fed indoors.

CPW recommends development of a Blue River Flats reclamation and weed managements plan that includes removal and control of non-native weeds during the construction period, annual monitoring and control post-development, and reseeding of disturbed areas to maintain health vegetation and prevent further spread of weeds along the Blue River corridor. CPW advises that the Blue River Flats HOA develop a Prohibited Plant List that includes all plants on (Appendix B). Several species on this list have been identified locally and are known to be present in Summit County (Appendix C)."

*RESPONSE: The site plan is currently showing a wetlands buffer that is setback off of the river, and do not have any permanent disturbances in this area. The paved path and community green are essential elements on the development, and have also been requested by other agencies to remain in place. Blue River Flats HOA agrees with the pet and trash policies and will work with the Town to put those mandates in place. Type of trees and plantings are available on the landscape sheets of the submittal package, and ensure that no noxious species will be planted on site.*



Godden|Sudik  
ARCHITECTS

July 12, 2016

Lina M. Lesmes  
Senior Planner  
Town of Silverthorne  
970-262-7366

**RE: Utility Undergrounding for Blue River Flats**

Dear Lina,

This letter is a detailed response to the Town's condition of approval number 16.4. Our team has now had the chance to thoroughly review the function of the power line that crosses the river, the logistics of undergrounding it under the river and alternatives to undergrounding and are unable to find a resolution that removes the overhead lines while fitting within the projects budget. We regret not being able to meet this condition but still hope to gain approval (staff and/or council) to see Blue River Flats closing units in 2017. Please advise us on how we shall proceed as timing is critical for project feasibility. All details of our exploration are listed below and/or attached to this letter. We are excited to move forward with the Blue River Flats and believe it would be a quality addition to the town of Silverthorne and a great northern anchor for the Town's River Front district.

Condition 16.4) All new, relocated and existing utility lines, including transmission lines, within the property limits need to be undergrounded. Since the eastern property line is within the river, the undergrounding and power pole relocation should terminate on the east bank of the river, on Town-owned Lot 39.

Background:

- The overhead electrical line is not needed to provide electrical service to Blue River 50. Instead, it connects a general distribution loop for the Town and is a service to numerous properties, not just the Blue River Flats – see attached *Blue River Loop Exhibit*.
- While we understand the request to underground the electric lines is stated as a condition, the Town code isn't absolute in its wording, stating in section 3-5-1 that it is "desirous" to underground utility lines where possible.

Proposed Design Solution: Underground all lines on site (outside the river) and overhead the river.

- The proposed solution is to install a new power pole near the east edge of the Blue River Flats site. That pole and guy wires location is coordinated to not interfere with the proposed improvements and the pedestrian path. Electrical lines west of that pole will be underground while electrical lines to the east and over the river will be above ground. Underground electrical lines will also be installed along the entire western property allowing easy future connection. This is expected to cost roughly \$70,000.00. Reasons this is the proposed solution include:
  - The additional costs associated with the other options.
  - The additional time required to design and process the other options. Per PSC and Xcel, this is estimated at 4-6 months, plus till the end of the Blue River moratorium ending in May, 2017. Amongst others, it includes the significant steps of a new survey to be conducted by the Army Corps of Engineers and another FEMA permit.

Godden|Sudik Architects

Plaza Québec  
6025 S. Quebec St., Suite 375  
Centennial, Colorado 80111

ph 303.455.4437  
fx 303.477.5680  
goddensudik.com

Option 1: Extending the loop north to the Tammy Jamison Bridge – (Likely the best option)

- The best option for removing the electrical lines over the river would be to cross the electric service at the Tammy Jamieson bridge at 13<sup>th</sup> St. There is already a feeder line running on the east edge of Highway 9 to near the southern edge of the Town's property and the feeder line could be mounted to the underside of the Tammy Jamieson bridge. About 750' of new feeder line would need to be installed, mostly along the east side of the river. It is likely parts of the existing bike path on the east side of the river would have to be torn up and replaced but it is expected to be a small amount (+/- 25%) due to the wider tract of land owned by the Town in this area.
- Xcel also suggested the overall benefit could also expand because the Town could expand the project to underground the power lines on the east side of the river further to the south to include the 4 houses where the feeder goes right over their roofs and decks to where it turns east to meet up with Mesa Dr. however, this would require some additional easements from the existing homeowners.
- Costs for this option are estimated to be between \$200,000.00 and \$250,000.00

Option 2: Constructing a new bridge at the current location

- Per an Engineer's Opinion of Probable Cost prepared for a similar bridge in 2014, this option is expected to cost +/- \$500,000.00.
- Based on our visit with the Tennis Court neighbors, any disturbance on their side of the river is not desired. Without a benefit of an extended project that undergrounds more wires, it was clear from them that they were not interested in any bridges / disturbance / etc. from this project.
- This cost is not feasible for the Blue River Flats project but is seemingly a better alternative than option 3 because for a similar cost, you get the added benefit of a usable pedestrian bridge.

Option 3: Undergrounding at the current location

- This option as initially requested by the Town is the costliest and most challenging logistically, thus it is listed as the last option.
- It is logistically challenging for the following reasons:
  - On the east side of the river, there is < 20 feet from the river edge to the pole / bike path. On the other side of the bike path is houses. This will be very difficult and very expensive to bore at this location – per Xcel.
  - The bore would normally need to be at least 5' beneath an irrigation ditch, probably deeper for a river (Corps question). It will take a larger than normal bore machine. It still may not be possible to achieve the depth required without the machine placed farther back from the river and you face the same situation bringing it back up on the other side. A channel bore (straight bore between two deep bore pits may work as long as dewatering (permit required and slow to get) can keep up.
- Based on our visit with the Tennis Court neighbors, any disturbance on their side of the river is not desired. Without a benefit of an extended project that undergrounds more wires, it was clear from them that they were not interested in any bridges / disturbance / etc. from this project.
- Attached is a cost estimate that projects this option to cost +/- \$500,000.00 - \$700,000.00

Respectfully,

  
Paul M. Brady, NCARB/AIA  
Principal

## MEMORANDUM

Date: 07/19/16  
To: Lina Lesmes  
From: Ken Marsh

Re: Bridge construction, power pole and utility line relocation

In exchange for the support and advocacy of the Town (Planning, Public Works and Engineering departments) BR 50 is willing to dedicate a tract of land to be known as the River Tract and grant an easement that will be used to accommodate a future pedestrian bridge, the removal of the power pole and facilitate the utility line relocation across the Blue River.

Two things are accomplished with this deed and dedication. First, the community walking paths and trail system allows for access across the River specifically facilitating easy, safe access from the west side of the River to the east and visa versa and Secondly, the power pole and overhead power lines are eliminated.

For this support BR 50 is also proposing a financing agreement or sinking fund type arrangement that includes a deed restriction allowing for a 1% transfer fee with the exception of the first sale of each unit (46). So using this 1% transfer fee, the Town 1% Xcel credit, the Town general fund and the Town capital improvement fund the Town can install the bridge and relocate the power pole and utility lines at their discretion.

## Reasons to support the First Sale 1x Exemption

1. 2016 residential real estate prices increased 14 % over 2015 in Silverthorne. The velocity of sales combined with huge price increases lead to a huge increase in RETA fees for the Town. As well, BR 50 is proposing that the RETA fees be set in perpetuity which will generate fees not only for burying the existing power line and building the bridge but will provide for an on going revenue stream for the Town, long after these improvements are paid for.
2. The smaller size of the units allow Silverthorne to provide an entry level housing product for young families and local residents that find it presently difficult to find housing in Summit County. Buyers of these smaller units typically outgrow these small units for a number of obvious reasons including growing families and career advancement, leading to property sales and rapid turns of units.
3. Flipping units often happens in low inventory, strong markets and increased sales often happen in a soft economy and bad markets especially in resort communities. History has shown there can be very quick multiple re-sales of the same properties providing repeat RETA fees to the Town of Silverthorne in as little as 1 or 2 years after the initial sale.
4. In order to provide and an affordable product, every \$ counts toward the cost of developing these units and this is a primary reason to consider allowing a 1x 1st sale RETA exemption. The RETA fee is a hardship when trying to provide affordable housing for young professionals and the locals of Summit County.
5. The origin of the discussion was to provide for the burial of the power lines not to construct a bridge. BR 50 is now on board with the concept of building a bridge and providing a way to pay for it through the RETA tax with the 1x 1<sup>st</sup> sale exemption.
6. The bridge will primarily and ultimately serve not only the Blue River 50 Community but all of the residents and visitors that use the trail system in Summit County, so the cost of constructing the bridge should not only be borne by the BR 50 community.
7. The bridge can be built with funds generated over a longer, but reasonable time period with the burden of the funding spread among more sellers in BR 50 and possibly from other funding sources available to the Town.
8. Additional fees increase overall costs that jeopardize the viability of the project and runs counter to the affordability objective given the fact that BR 50 is providing a higher quality product and is striving to stay affordable.
9. Keeping BR 50 a financially viable community will immediately contribute to the beautification of the Blue River Parkway and this riverfront area of Silverthorne.

**TO:** Lina Lesmes – Senior Planner, Community Development Department  
**FROM:** Dan Gietzen - Town Engineer, Public Works Department  
**DATE:** September 28, 2016  
**RE:** Blue River 50 Final Site Plan review comments

---

**1) Development within the floodplain and FEMA permitting.**

Most of the project site is located within the FEMA mapped 100-year floodplain.

Development within the floodplain is permissible as long FEMA, State and local standards can be met. The two main criteria include 1) elevating the lowest floor elevations to be at least one (1) foot above the FEMA mapped and published Base Flood Elevations (BFEs), and 2) performing scientific analysis and modeling to evaluate the effects that proposed fill would have on water surface elevations. Per State and local standards, development is allowed as long as it can be demonstrated, via scientific modeling, that changes to BFEs are equal to or less than 0.5 feet.

The Applicant provided a floodplain analysis performed by Wright Water Engineers (WWE), dated February 15, 2016. The study concluded, that the post-project maximum change to the floodway water surface elevations would be 0.29 feet higher, which is less than the 0.5 foot threshold – and thus allowed by FEMA, State and local regulations.

FEMA has since reviewed and approved the study and has issued the CLOMR-F permit, which stands for “Conditional Letter of Map Revision based on Fill.” This permit is FEMA’s official approval of the proposal to fill the site for development per the current site plan. Silverthorne Staff required the CLOMR-F as a condition of approval at Preliminary – and this has been satisfied.

Prior to the issuance of any Building Permits, the site will need to be filled to match the footprint and vertical elevations specified in the FEMA approved CLOMR-F. The Applicant will then need to submit the “as-built” site conditions to FEMA as part of the LOMR-F application process. FEMA will need to review and approve the application which documents and verifies as-built conditions, and will need to issue the LOMR-F permit, which officially removes from the FEMA regulatory floodplain, before any Building Permits can be issued.

With respect to building elevations, all are shown to meet or exceed the one (1) foot minimum elevation above the BFE. This will also be confirmed by survey and Elevation Certificates during construction.

**2) Proposed encroachments into Xcel easements.**

The site is encumbered with several different easements. Of particular interest are two (2) Xcel utility easements which proposed buildings are shown to encroach into. At Preliminary review, Silverthorne staff required that the Applicant obtain permission from the easement Owner, Xcel for these encroachments in order for to move forward to Final. The Applicant has since provided an email from Xcel stating that Xcel has no objections to the proposed encroachments.

### **3) Access / CDOT Access Permit**

The Applicant has received from CDOT and has provided to the Town two (2) Access Permits for the project – one for each the northern and southern access points. CDOT has accepted the current access points and configurations and is not requiring any additional highway improvements such as accel/decel lanes, turning movement restrictions or anything else.

### **4) Grading and Drainage**

Staff has reviewed the Grading and Drainage Plan, and finds that it meets the Town's requirements.

### **5) Sidewalk and Landscape Wall proposed within CDOT right-of-way**

CDOT permission and the appropriate permitting for these improvements will be required. The Applicant has submitted a Special Use Permit Application to CDOT, which is currently being reviewed by CDOT. CDOT must issue a permit for these improvements for them to be constructed.

The sidewalk will be privately maintained until such time that it connects to a larger, contiguous Town path system.

It is recommended that the sidewalk be at least a few feet away from the buildings for snow maintenance / plowing activities.



TO: Lina Lesmes, Senior Planner  
FROM: Zach Margolis, Utility Manager  
DATE: September 29, 2016  
SUBJECT: Utility Department Comments on Blue River Flats Final Site Plan

**General:** The Town of Silverthorne has sufficient water and sewer capacities in the adjacent right of way to serve the proposed project. We have no recommended conditions of approval.

We would like a final set of construction drawings at least ten days prior to start of construction. We ask that the following notes and details be included on the construction drawing set:

- Town of Silverthorne Details, S-1, S-2, S-3, S-4, S-6, S-7, W-1, W-2, W-3, W-4, W-5, W-7, W-8, W-9, and W-13.
- Water Note 1 - Add "DR 18 (245 psi) C900 PVC" to.
- Add the following Water Note - All water services shall be Pure Core DR 9 HDPE.
- Add the following Water Note - All restraint steel and hardware is to be 304 stainless steel.
- Add the following Sewer Note - All sewer services and cleanouts shall be SDR 26 PVC.
- Typical Insulation Detail - change the bottom left hand note to read, "3/4" Crushed Rock".

**SILVERTHORNE TOWN COUNCIL**  
**Meeting Minutes**  
**Wednesday, September 14, 2016**

**CALL TO ORDER/ROLL CALL/APPROVAL OF AGENDA:**

Those members present and answering Roll Call were Mayor Bruce Butler, Council Members, Peggy Long, Russ Camp, Bob Kieber, JoAnne Nadalin and Tanya Shattuck. Derrick Fowler absent. Staff members present were, Town Manager Ryan Hyland, Chief of Police John Minor, Recreation Director Joanne Cook, Public Works Director Bill Linfield, Finance Director Laura Kennedy, Assistant Town Manager Mark Leidal, Senior Planner Lina Lesmes, Greg Roy Planner I, Town Attorney Matt Mire and Town Clerk Michele Miller.

The Pledge of Allegiance was recited by those present.

**STAFF COMMENTS:**

Cook updated Council on the Recreation Center Annual Shutdown. The Silverthorne Pavilion will host a Café Concert next Thursday night.

**COUNCIL COMMENTS:**

None.

**CITIZEN COMMENTS:**

Terry Matthew, 233 Hummingbird Circle, read a prepared statement regarding his opposition to issuing business licenses to short term rentals in residential neighborhoods. He reviewed the negative impact short term rentals have on him and his neighbors.

**CONSENT CALENDAR:**

**SHATTUCK MOVED TO APPROVE THE MINUTES FROM AUGUST 24, 2016. MOTION SECONDED. MOTION PASSED UNANIMOUSLY BY COUNCIL PRESENT (FOWLER ABSENT).**

**NADALIN MOVED TO APPROVE THE CONSENT CALENDAR INCLUDING RESOLUTION 2016-26 - A RESOLUTION AWARDDING THE CONTRACT FOR THE EAST 4<sup>TH</sup> STREET SIDEWALK EXTENSION PROJECT TO OROZCO CONCRETE INC. OF DENVER CO., RESOLUTION 2016-27 - A RESOLUTION AWARDDING THE CONTRACT FOR THE WARREN AVENUE DRAINAGE IMPROVEMENTS PROJECT TO MOUNTAIN DIGGERS CORPORATION OF SILVERTHORNE, CO., RESOLUTION 2016-25 - A RESOLUTION AWARDDING THE CONTRACT FOR THE ADAMS AVENUE SIDEWALK CONSTRUCTION PROJECT TO OROZCO CONCRETE INC. OF DENVER CO. MOTION SECONDED. MOTION PASSED UNANIMOUSLY BY COUNCIL PRESENT (FOWLER ABSENT).**

**PUBLIC PRESENTATIONS:**

**Summit School District**

Kerry Buhler, Superintendent, introduced members of her team, Bonnie Ward, Cindy Bargell, Sue Wilcox and Kara Drake. Buhler reviewed the new Comprehensive Facilities Master Plan project, the Supplemental Capital Construction, technology and Maintenance Mill Levy and Bond Issue – Pursuing critical repairs and renovations to school buildings. The ballot questions 3A & B will be on the upcoming November ballot. She requested support. She stated that the

School District was requesting municipalities consider a adopting resolution stating their support of the mill levy and both bond questions.

**LIQUOR BOARD:**

- A. **Pizza Hut of SW Kansas, Inc. dba Pizza Hut – Renewal of 3.2% Beer On Premise Liquor License**
- B. **Suncor Energy Sales, Inc. dba Shell – Renewal of 3.2% Beer Off Premise Liquor License**

**NADALIN MOVED TO APPROVE**

- **PIZZA HUT OF SW KANSAS, INC. DBA PIZZA HUT – RENEWAL OF 3.2% BEER ON PREMISE LIQUOR LICENSE**
- **SUNCOR ENERGY SALES, INC. DBA SHELL – RENEWAL OF 3.2% BEER OFF PREMISE LIQUOR LICENSE.**

**MOTION SECONDED. MOTION PASSED UNANIMOUSLY BY COUNCIL PRESENT. (FOWLER ABSENT)**

**PUBLIC HEARINGS:**

- A. **Minor Subdivision – Verizon Cell Tower, 2700 Golden Eagle Road, Eagles Nest Golf Course, Filing No. 6 First Amendment**  
Public hearing opened.

Matt Mire, Town Attorney noted Home Rule Charter, 3.9, Conflict of Interest, states only the Council as a whole can vote on whether a Council member has a conflict under Charter 3.9. The Council determines if there is a substantial personal or financial interest or not. The affected Council member does not vote on the question of whether a conflict exists. He asked if any Councilmember had anything to declare.

JoAnne Nadalin Councilmember declared her participation in the Osprey Reserve HOA and noted she has had a lot of a communication regarding the Verizon Cell Phone Tower in this capacity. She feels that she has prejudged the application.

Long declared that she is the bookkeeper for the Eagles Nest HOA and in that capacity is responsible for sending out e-mails to the Eagles Nest HOA membership encouraging participation in everything from volunteer weed clean up days, to attending at the Verizon cell phone public meeting.

Mire asked if she has prejudged the Verizon Cell Phone application.

Long stated no; she does not know anything about the project, but she is looking forward to hearing about it.

Camp stated his wife is a member of the Eagles Nest HOA and some of the HOA board conversations have been repeated at home.

Mire asked Camp if he feels that he has prejudged this application.

Camp stated he has not prejudged this application.

**LONG MOVED TO FIND THAT COUNCILMEMBER NADALIN HAS A CONFLICT OF INTEREST IN THE VERIZON CELL TOWER MATTER AND THEREFORE SHOULD BE EXCUSED FROM VOTING ON AGENDA ITEM, PUBLIC HEARINGS, A. AND ACTION ITEM**

**A. AS OUTLINED IN 3.9 OF THE HOME RULE CHARTER. MOTION SECONDED. MOTION PASSED UNANIMOUSLY BY COUNCIL PRESENT. (FOWLER ABSENT) (NADALIN ABSTAINING)**

**NADALIN MOVED TO FIND THAT COUNCILMEMBER LONG DOES NOT HAVE A CONFLICT OF INTEREST IN THE VERIZON CELL TOWER MATTER. MOTION SECONDED. MOTION PASSED UNANIMOUSLY BY COUNCIL PRESENT. (FOWLER ABSENT)**

**LONG MOVED TO FIND THAT COUNCILMEMBER CAMP DOES NOT HAVE A CONFLICT OF INTEREST IN THE VERIZON CELL TOWER MATTER. MOTION SECONDED. MOTION PASSED UNANIMOUSLY BY COUNCIL PRESENT. (FOWLER ABSENT)**

Nadalín left the Council Chambers.

Greg Roy, Planner, I, presented the Applicant's request for a Minor Subdivision approval to create an easement for their proposed telecommunications facility and the Site Plan Modification allowing a new telecommunications facility to be located on Eagles Nest Property Homeowners Association property. He reviewed the staff report for both the Public Hearing Item A. and the Action Item A. and recommended approval for both.

Brad Johnson - Black and Veatch, representing the applicant, explained the site and service area. He reviewed their application. They have worked closely with the Eagles Nest HOA to be consistent with the design, colors, etc. He cited cell phone use statistics.

Council questions.

Council asked about the increase in cell phone coverage with the proposed tower.

Johnson reviewed cell phone coverage map with Council.

Butler asked about the trees being planted on the site, he asked if a taller tower would be more helpful.

Johnson stated an increase in height would probably help, but he wants to see how this tower works as presented. They might be back to ask for additional height later.

Kieber asked if cell phone panels might work better than a new tower. He asked if there were any sites looked at in Angler Mtn. Ranch.

Johnson reviewed the site selection. They were looking for a centrally located tower.

Kieber asked about the proposed building, servicing, parking and diesel stored on site.

Johnson stated the diesel on site is for the generator in case of emergency. He reviewed the radio frequency transmission from the site.

Camp asked about cell phone radio frequencies (RF) from cell phones and cell phone towers.

Bryan Eicens, Verizon Wireless RF Engineer reviewed (RF) from cell phones and cell phone towers. There is more exposure to RF with your personal cell phone, than a cell phone tower.

Johnson reviewed the RF of cell phones; this site is monitored 24/7 by the company.

George Resseguie - 1770 Red Hawk Circle, Eagles Nest HOA President, explained how the project was reviewed and met the requirements from the HOA and the Town Code. The Design Review Committee oversees the maintenance and appearance of the project. The HOA has had several meetings on the issue; very few people didn't want it. He feels this is a good project or he wouldn't bring it to the Town for approval.

Citizen comments.

Gary Beisler, 2912 Osprey Lane, expressed his concerns about the proposal. He doesn't like diesel fuel being stored on the site. He doesn't think this tower should be located in a residential area. He would like to see an alternative to the diesel generator. He would like to

September 14, 2016

Page 3

see cell phone coverage extended further north of town and if this tower doesn't do that it seems half baked.

Long asked how much diesel is stored on site.

Johnson stated around 200 gallons. There are other cell towers located in residential areas in Colorado.

Public hearing closed.

Council Comments.

Long stated times are changing and everyone has at least one cell phone. This is the new utility in modern times. She supports this and hopes it increases cell phone coverage further north. She feels like this is a good attempt to fix an issue. Diesel is less combustible than other gases. She is glad the HOA has given its stamp of approval. She supports the project.

Camp stated in years past he has heated his home with diesel and never had an issue. He likes the idea of improved cell service. He supports the project.

Shattuck likes the project and hopes that the tower increases coverage further north.

Kieber stated he is concerned about the use of diesel on site and the road up to the tower in winter driving conditions. He would prefer to see natural gas as the backup and he has concerns about parking. He will not support the project.

Butler stated he grew up in a home where home heating oil was the heating source. If the HOA is granting permission, he would be reluctant to contradict them in their decision making process. This project makes sense for the community.

**CAMP MOVED TO APPROVE THE PROSED MINOR SUBDIVISION. MOTION SECONDED. MOTION PASSED BY COUNCIL. (KIEBER NAY) (FOWLER ABSENT) (NADALIN ABSENT AND EXCUSED FROM VOTING)**

**ACTION ITEMS:**

**A. Site Plan Modification – Verizon Cell Tower, 2700 Golden Eagle Road, Eagles Nest Golf Course, Filing No. 6 First Amendment**

**CAMP MOVED TO APPROVE THE SITE PLAN MODIFICATION FOR THE NEW CAMOUFLAGED WIRELESS TELECOMMUNICATIONS FACILITY. MOTION SECONDED. MOTION PASSED BY COUNCIL. (KIEBER NAY) (FOWLER ABSENT) (NADALIN ABSENT AND EXCUSED FROM VOTING)**

Nadalín returned to the Council Chambers.

**B. Preliminary Site Plan – McDonald's, 220 Summit Place**

Lina Lesmes, Senior Planner, presented the applicant's request for approval of a Preliminary Site Plan to construct a new 5,265 square foot McDonald's restaurant in the C-1 Zone District and the Gateway Design District. She reviewed the staff report and requested approval with the Planning Commission's ten conditions of approval.

Council Questions.

Nadalín asked about split cedar, there are a lot of Fire Departments that don't allow it to be used.

Lesmes stated the Fire Department has reviewed the plans. She reviewed the snow storage.

Long asked if the footprint is expanding or just going up.

Lesmes stated the footprint is bigger.

Butler stated when the Design Standards were reviewed; they looked a lot of different things for this area, landscaping included. He thinks there is too much landscaping. He requested the

Applicant not plant plants that grow up and block views when driving. He requested that Staff and the Applicant seriously consider a reduction in landscaping requirements.

Lesmes stated the Applicant was given landscaping credit for their xeriscaping, fence and planter boxes.

Council discussed landscaping.

Kieber asked about the signage calculations.

Lesmes reviewed the sign calculations. The Applicant will be relocating the current free standing sign. Their sign variance was personal to the Applicant and it has no expiration date.

Robert Palmer, Strategic Land Solutions, representing the Applicant, complimented Lina on the good job she does for the Town and Applicants. The building size increase has to do with the play area. He addressed Council's landscaping concerns. He reviewed the snow storage; he feels it may be miscalculated. He talked about how the building will be bigger and still work on the site. He explained the drive isle configuration. He addressed the drainage reconfiguration. The Applicant listened to Council's comments at the previous submittal. They held their community meeting and have met all the Town Code requirements. He requested approval.

Nadalin asked about the flow of the traffic on the site.

Kieber asked about the trash enclosure and the size of the dumpster. He asked when the project would be started.

Butler asked the applicant if they can meet the conditions of approval.

Palmer stated yes.

Butler disagrees with landscaping requirements and supports striking condition number four. He thanked the Applicant for including the play area in the design.

**KIEBER MOVED TO APPROVE THE MCDONALD'S PRELIMINARY SITE PLAN WITH THE FOLLOWING PLANNING COMMISSION RECOMMENDED CONDITIONS:**

1. THAT A MINOR SUBDIVISION PLAT BE SUBMITTED WITH THE FINAL SITE PLAN SHOWING THE LOCATION OF THE WATER LINE EASEMENT AND ANY ACCESS EASEMENT.
2. THAT A CROSS-ACCESS EASEMENT AGREEMENT BE SUBMITTED WITH THE FINAL SITE PLAN FOR THE STAIRWAY SHARED WITH LOT 1, SKELLY REDEVELOPMENT SUBDIVISION.
3. THAT SNOW STORAGE AREAS EQUAL 25% OF THE PAVED PARKING AREAS, AND BE LOCATED WHERE THEY FACILITATE THE SNOW STACKING PROCESS.
4. THAT THE LANDSCAPE PLAN BE REVISED TO MEET THE MINIMUM STANDARDS OF SECTION 4-6-11.
5. THAT ALL PROPOSED LANDSCAPE ISLANDS MEET THE REQUIREMENTS OF SECTION 4-6-11.G.20.
6. THAT THE LIGHTING PLAN BE REVISED SUCH THAT ALL EXTERIOR LIGHT FIXTURES ARE FULLY SHIELDED FIXTURES, WITH DOWNLIGHT LIGHTING ONLY.
7. THAT ALL UTILITY, TELECOMMUNICATIONS, GROUND MOUNTED, AND ROOF-TOP MECHANICAL EQUIPMENT BE SHOWN ON THE FINAL SITE PLAN.
8. THAT THE EXTERIOR MATERIAL OF THE RETAINING WALL BE SUBMITTED, MEETING STANDARD 3.5.5 OF THE GATEWAY DISTRICT DESIGN STANDARDS.
9. THAT THE 'TRADITIONAL RED' COLOR PROPOSED FOR THE 'HARDI-BOARD' SIDING BE REVISED SUCH THAT IT DOES NOT EXCEED A CHROMA OF 4.
10. THAT ALL THE REQUIREMENTS OF THE UTILITIES MANAGER BE ADDRESSED WITH THE SUBMITTAL OF THE FINAL SITE PLAN.

**MOTION SECONDED. MOTION PASSED BY COUNCIL PRESENT. (FOWLER ABSENT)  
(LONG NAY)**

**C. Site Plan Modification – J&H Auto Service, 205 & 207 Warren Avenue, Lot 2-A, Block 2, Enterprise Park Subdivision.**

Greg Roy, Planner I, presented the Applicant's request for a Site Plan Modification to construct a building addition for storage. He reviewed his staff report and requested approval.

Kamil Petrick - J & H Mountain Services, requested approval. He thanked Council for the new "No Parking" sign in front of his business.

Long thanked Petrick for the proposed changes and feels it should help with the issues he has been having.

**CAMP MOVED TO APPROVE OF THE SITE PLAN MODIFICATION, 205 AND 207 WARREN AVENUE, LOT 2-A, BLOCK 2, ENTERPRISE PARK SUBDIVISION.**

**MOTION SECONDED. MOTION PASSED UNANIMOUSLY BY COUNCIL PRESENT (FOWLER ABSENT).**

**DISCUSSION ITEMS:**

Long thanked Greg Roy for the list of code enforcement accomplishments over the last year, she thanked him for all of his hard work.

**INFORMATIONAL:**

- A. Planning Commission Meeting Minutes, September 6, 2016
- B. SPORT Committee Meeting Minutes, August 18, 2016

**LONG MOVED TO ADJOURN. MOTION SECONDED. MEETING ADJOURNED AT 8:19 P.M.**

---

**BRUCE BUTLER, MAYOR**

**ATTEST**

---

**MICHELE MILLER, TOWN CLERK**

These minutes are only a summary of the proceedings of the meeting. They are not intended to be comprehensive or to include each statement, person speaking or to portray with complete accuracy. The most accurate record of the meeting is the videotape of the meeting, maintained in the office of the Town Clerk.



**Economic Development Advisory Committee  
MINUTES**

**Tuesday, September 6, 2016  
Town Hall – 601 Center Circle**

**DRAFT**

- 1. Call to Order – 12:00 noon**  
EDAC - Mike Shambarger, Les Boeckel, Linda Hrycaj, Larry Lunceford, Ken Gansmann, Jayne Esser, Don Hansen, John St. John, Larry Lunceford, Marc Hogan, Scott Wilson  
Council - Russ Camp, Bruce Butler  
Staff - Ryan Hyland, Mark Leidal, Lina Lesmes, Laura Kennedy, Susan Schulman  
Guest - Kelly Flenniken – Xcel Energy
- 2. Approval of Minutes –** Ken Gansmann made a motion to approve, Les Boeckel second. Minutes from July 5, 2016 pass unanimously.
- 3. Guest Presentation-** Kelly Flenniken, Xcel Energy Area Manager for Community and Local Government Affairs. Kelly started with Xcel Energy 18 months ago, and was previously Executive Director and Business Development Manager for the Grand Junction Economic Partnership. Kelly currently serves as the Chair for the Economic Development Council of Colorado. Kelly discussed Xcel's current 5 year business plan which includes a focus on innovative clean technology such as natural gas, wind and solar energy; and how these technologies will benefit customers in the future. In Silverthorne, Xcel has 35 full-time employees; Xcel wants to be a good business partner and become more efficient for local developers. Xcel has built high speed internet into Silverthorne's Summit Sky Ranch and attends quarterly meetings of the Summit County Utility Coalition along with CDOT, Municipalities and other Stakeholders. Xcel is a sponsor of the upcoming Colorado Municipal League District 12 Annual Meeting hosted by the Town of Silverthorne at the Pavilion, and the Silverthorne Elementary School Bear Strong Bolt 5K fun run.

Mayor Bruce Butler followed up on the service aspect suggesting face to face office hours with customers to be held at Town Hall.

Les Boeckel suggested a public seminar with Summit County Builders Association to improve efficiency for local developers. Kelly responded that Xcel is considering public service announcements on SCTV for developers as well as current Xcel customers.

4. **Town Core Catalyst Site**- Ryan Hyland updated EDAC on the Town's intent for revitalization and redevelopment of the Town Core Catalyst Site – Old Dillon Inn, Mint, and Mountain Lyon Café block. A draft RFP has been sent to local developers for feedback before it goes out to the public. The Town would like to make the RFP public sooner rather than later in order to dispel rumors. The 3 property owners of this site are of like mind.
5. **Sign Code Revision** – Lina Lesmes presented a power point presentation and reminded EDAC that the Sign Code sub-committee began this project in March 2016 and 5 meetings of the sub-committee have been held. Two community meetings are coming up in October 2016 to gather public input which will include attorney input based on a recent municipal lawsuit. The intent is to have a new Sign Code Ordinance approved by Council in the spring of 2017.
6. **EDAC Business Grant Updates** – Susan Schulman discussed the fact that \$10,000 of the Business Grants funds has gone unused in 2016. The EDAC Executive Committee, at the approval of Council, will re-open the Business Grant Program in order to allocate these funds in 2016. The program has been announced in the Summit Daily and on the Town's Website and Social Media. Applications are due Monday, September 12 and the Committee will review applications the week of September 19, with recommendations being presented to Council at the September 27 Work Session.

#### 7. **Staff Updates**

Laura Kennedy reported on sales tax through June 2016; numbers are up over the year. It is notable that tax collected reached over \$1 million in June 2016 and is on track for the same in July 2016. In the past, the \$1 million mark has only been reached during the winter holiday period.

Ryan Hyland reported on the grand opening at Sauce on the Blue, and the upcoming opening at Studio B Dance Center.

Ryan also reported on the November Housing ballot item which is intended to fund workforce housing through a sales tax. An Open House on September 7 for the Lake Hill Neighborhood Master Plan for local workforce housing will be held at the Community Center in Frisco. Lina Lesmes is attending this meeting on behalf of the Town. Marc Hogan feels an I-70 overpass is necessary for this community and should be addressed in the Master Plan.

8. **Adjourn** – 1:40 p.m.
9. **Next Meeting** – November 1, 2016 at 11:45 a.m. in the Schmidt Room